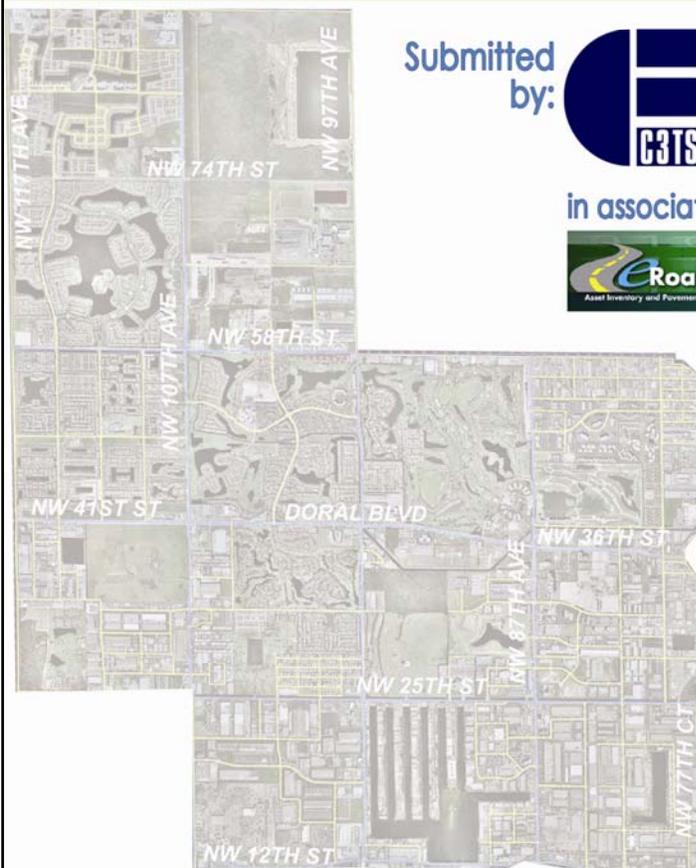


CITY OF DORAL

PAVEMENT EVALUATION 2011 AND FIVE-YEAR MAINTENANCE & REHABILITATION REPORT



JANUARY 2012



Submitted by:



in association with:





PROFESSIONAL ENGINEER CERTIFICATE

I hereby certify that I am a registered professional engineer in the State of Florida practicing with C3TS, P.A., a corporation, authorized to operate as a an engineering business (EB 5022), FEID No. 65-0039493, by the State of Florida, Department of Professional Regulation, Board of Professional Engineers, and that I have reviewed or approved the evaluation, findings, opinions, conclusions, or technical advice hereby reported for:

Project: DORAL PAVEMENT EVALUATION 2011 AND FIVE-YEAR MAINTENANCE & REHABILITATION

FIN: N/A

C3TS No: 00426-011-0001-03

Location: Miami Dade County, Florida

Client: City of Doral

This Engineering report includes a summary of data collection efforts and an engineering analysis for the City of Doral Pavement Evaluation 2011 and Five-Year Maintenance & Rehabilitation. I acknowledge that the procedures and references used to develop the results contained in this report are standard to the professional practice of engineering and planning as applied through professional judgment and experience.

Name: Dave E. Clarke, P.E.

Signature: _____

P.E. Number: 66553

Date: _____



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1.0 EXECUTIVE SUMMARY

This report provides an overall assessment and evaluation of the pavement condition of streets maintained by the City of Doral. M&R plan...C3TS recommends Major M&R. Results from this evaluation was then used developed a Five Year Maintenance and Rehabilitation Plan.

C3TS recommends Major M&R for the City of Doral's AC pavement roadway network based on knowledge and professional judgment associated with PMS. Recommended work activities for M&R shall consist of complete pavement reconstruction, milling and resurfacing (1-inch), and 1-inch overlay.

Work plan and Budget analysis was performed for in Paver™ based on the Minimum PCI and Critical PCI. The minimum PCI condition began by maintaining an annual PCI 86 but was later varied to reduce the high budget cost in the first year. The critical PCI method used maintained the current area weighted PCI using a critical PCI of 70. One major disadvantage of this critical PCI method is that major critical projects where PCI are below critical are given low priority. The minimum critical PCI method also included planned work for fiscal year 2011/2012 which the City of Doral wanted to include.

Overall the cost of the variable minimum PCI was approximately \$8.8 million over the 5 years with about 3.8 required in the first year. The average weighted PCI after work is completed under this plan is 86, 93 92, 94 and 93 for 2012, 2013, 2014, 2015 and 2016 respectively.

The total cost of the Critical PCI method is approximately \$5.2 million with approximately \$1.9 million required in the first year. The average weighted PCI after

work is completed under this plan is 86, 87, 86, 85 and 83 for 2012, 2013, 2014, 2015 and 2016 respectively.

2.0 INTRODUCTION

The City of Doral, located in Miami-Dade County Florida, was incorporated on June 24, 2003, and is home to approximately 46,000 residents. One of 35 municipalities throughout the County, the city was ranked 51st in "The "Top 100 Places to live and Launch a Business in the United States" by CNN Money and Fortune Small Business in 2008. This unique and attractive quality, mixed land use, and close proximity to Miami International Airport makes the City of Doral one of the most vibrant and fastest growing Cities in the Country, regularly hosting in excess of 100,000 people who work within the Cities. Doral occupies a land area of approximately 15 square miles and maintains more than 57 miles of existing roadway network. The City's aptitude for attracting residents and business alike necessitates the need for a reliable Pavement Management System (PMS) to ensure a smooth riding surface at all times. This will allow the City to predict future pavement conditions and strategically identify cost effective maintenance and rehabilitation (M&R) projects.

2.1 Consultant Role

C3TS was retained by the City of Doral to perform automated field data collection along all City owned road (57 miles) with image capture photos every 50-ft for asset management. This data will be used to determine the Pavement Condition Index (PCI) based on current conditions and develop a PMS utilizing a software called Paver™ 6.5 developed by the US Army Corps of Engineers. ERoadInfo, a sub consultant for C3TS, conducted the data collection and populated the results into Paver™ to determine the PCI for the existing roadway sections.



2.2 Pavement Management Systems Concept

A PMS provides a power tool to assist Municipalities in making decisions regarding M&R planning and implementation. Successful pavement management requires scheduling of M&R during the appropriate stages of deterioration before to optimize budgets and overall performance. Figure 1, taken from "Pavement Management for Airports, Roads and Parking Lots 2005", Second Edition by M.Y. Shahin, provides a conceptual illustration of how a pavement normally deteriorates and the associated cost of rehabilitation at various times throughout its life.

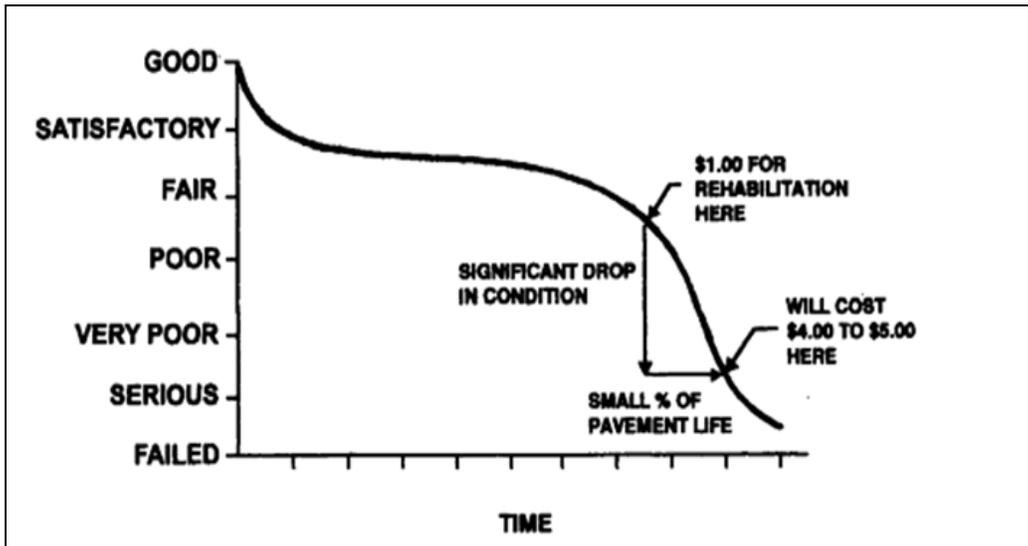


Figure 1-Pavement Life Cycle

The length of "satisfactory" years of a pavement is a direct correlation on how well the pavement has been maintained. Implementation of M&R before significant drop off in pavement condition can result in major cost savings over the life of the pavement. Another advantage of preventive M&R besides cost, is reduced traffic detours and lane closures associated with pavement repair beyond critical deterioration (complete reconstruction) which has a negative impact of business and residents. By predicting the deterioration rate of pavement along with using

standardized unit cost and minimum pavement performance requirement, a successful M&R Plan can be developed and implemented.

2.3 Purpose

The ultimate objective of this project is to survey all City owned roads, determine roadway conditions using PCI rating, and identify future five-year pavement M&R plan City of Doral. These objectives were accomplished through a series of systematic procedures which include:

- Pavement Inventory
- Data Collection
- Pavement Condition Rating
- Pavement Future Performance
- Prioritization of Repair & Rehabilitation
- M&R Projects

This report summarizes our findings and results of the 2011 Pavement Management System using Paver™ for the City of Doral.

3.0 METHODOLOGY

3.1 Roadway Network Inventory

As a starting point, a Geographic Information System (GIS) base shape file of Doral's road network was provided. The road network contained all City (Doral), Private (Gated Developments), County (Miami-Dade County), and State (Florida) owned roads including street names, ownership, and section length. Majority of the roads owned by the City have both curb and gutter and sidewalk consisting of one or two travel lanes in both directions. Also contained in the database was the classification

for each roadway section used in the Section Ranking. The database was modified as required to match current field conditions. Also the 2007 PCI was also updated into the GIS base file. This PCI information was contained in the 2007 PMS report conducted by Sanchez-Zeinali & Associates Inc. and their sub consultant Applied Research, Inc. (ARA). A map of all the current roads and ownership within the limits of the City of Doral can be found in Appendix A.

3.2 Data Collection

Data collection involved automated surveying of the existing roadway surface of approximately 57 centerline miles of roads owned by the City of Doral, Florida. Image capture of all City owned roads was taken every 50-ft. An additional 33 centerline miles of County owned roads was driven to capture roadway images that can be used for hyper-link method to view these images. The total mileage for the City and County roads is estimated to be approximately 100 miles.

3.3 Pavement Condition and Rating

The City of Doral has decided to use Paver™ software version 6.5 for their PMS which uses the PCI method to determine distress in pavement. Therefore, the PCI will be used to rate the present condition of the surface road network. The PCI is a subjective method of evaluating distresses in pavement by providing a numerical rating between 0 and 100 where 0 is worst and 100 is best condition. This rating is based primarily on the type, quantity and severity of distresses for each pavement section. This concept was developed by the US Army Corps of Engineers in the late 1970's and was adopted by the American Society for Testing and Materials (ASTM) as the standard for airfields in 1993 and roads and parking lots in 1999 as well as other agencies. Paver™ 6.5 uses the ASTM 2010 standard for new distress in pavement, ASTM D 6433-09 Standard Practice for Roads and parking Lots Pavement

Condition Index Surveys. The overall rating scale used in Paver™ for the PCI can be found below in figure 2. For this project, the standard PCI rating scale is used. Paver™ allows for customization of the standard color rating scale to for any needs

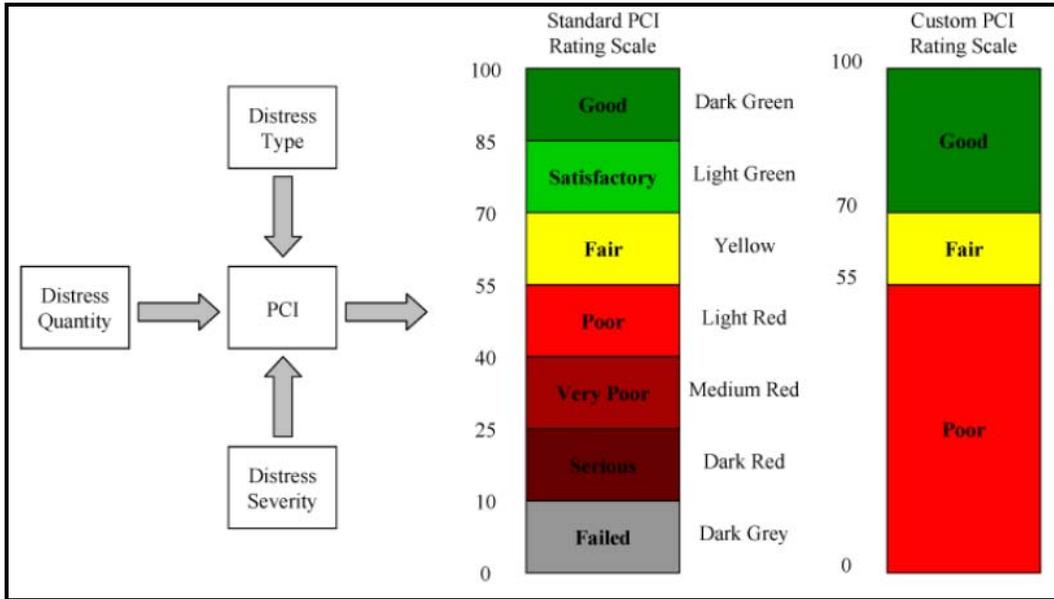


Figure 2-Pavement Condition Index Rating Scale

All pavements will deteriorate over time. This deterioration is typically a result of distresses that may be attributed to traffic applications/load, construction, materials, environment, and climate. In asphalt surface roads, the current ASTM standard for PCI currently recognizes pavement deterioration as a function of twenty distress type, three levels of severity (L-low, M-medium & H-high) and distress amount or density. A summary of these distresses, code, severity levels and causes are provided below in table 1 below.



Table 1-Asphalt-Surface Distress Classification for Roads and Parking Lots

Code	Distress	Severity Levels	Probable Cause
01	Alligator Cracking	L,M,H	Traffic
02	Bleeding	L,M,H	Materials, Construction
03	Block Cracking	L,M,H	Climate
04	Bumps and Sags	L,M,H	Climate, Traffic
05	Corrugation	L,M,H	Traffic
06	Depression	L,M,H	Construction
07	Edge Cracking	L,M,H	Traffic
08	Joint Reflection	L,M,H	Climate
09	Lane/Shoulder Drop-off	L,M,H	Other
10	Longitudinal and Transverse Cracking	L,M,H	Climate
11	Patching and Utility Cut Patching	L,M,H	Other
12	Polished Aggregate	N/A	Traffic
13	Potholes	L,M,H	Traffic
14	Railroad Crossings	L,M,H	Other
15	Rutting	L,M,H	Traffic
16	Shoving	L,M,H	Traffic
17	Slippage Cracking	L,M,H	Traffic
18	Swell	L,M,H	Climate, Environment
19	Raveling	M,H	Materials
20	Weathering	L,M,H	Climate

The condition survey was completed by eRoadInfo using a surface mobile data collection surveying vehicle that is equipped with cameras and various Global Position System (GPS) equipments. Prior to performing the inspection the roadway network was divided into manageable sections. Using the GIS base file, the sections were typically identified from intersection to intersections.





The E-RoadInfo Capture module offers digital roadway imaging at a 1280 x 960 resolution taken from up to 6 cameras (including our high resolution 11 mega-pixels camera) from the moving survey vehicle. The cameras are mounted in adjustable housings above the vehicle for easy access, best viewing angle, and protection from outside elements. The cameras are pointed forward, left side, and right side with right-of-way views. The field of view includes the lane of travel, street signs, guide signs, mile markers, pavement markings, billboards, vegetation, terrain, and overhead signs. The images from the cameras can assemble into a continuous 180 degree view. Additionally, two cameras are mounted on the rear of the vehicle to capture the reverse view. The camera positions are fully adjustable and can be locked once the desired position is acquired. Pavement inspection can be completed in real-time with 100% distance coverage at speed from 3 to 70mph.

Typically, the images are captured at 55 ft intervals. The images are stored as JPEG files. Each image is approximately 200kB. At 3 frames per 25 ft mile, the images will require about 120 MB per mile. For this project images were captured every 50-ft.

The images position and orientation data are captured into a GIS database along with the data. The data can be easily transported from the on-vehicle computer to a central network using wireless network or removable hard drives.

Main Features

- 360 Degree panoramic camera collecting data at 15 MP
- High definition (HD) camera with 11 MP 4000x2600 resolution images
- Synchronized data collection with up to 6 cameras
- Flexible camera locations and orientation





- Full-frame, non-interlaced JPG format
- Images are associated with Inertial Navigation System to acquire accurate location as well as camera heading, roll, and pitch information.
- Images are stored on hard drives in real-time. No post processing required.
- System automatically finds the street based on the GPS when capturing.
- System includes a progress tracking feature to verify all roads are covered.
- Capture is paused when vehicle stops moving.
- Distance information is automatically attached to all images.
- Bookmark and voice notes can be captured to provide additional information not easily visible from images.

ERoadInfo fully integrated data collection vehicle (figure 2) provides complete pavement data collection capability with a fully integrated pavement data collection vehicle that includes GPS/INS sensors, Right-of-way cameras, Longitudinal profiler (IRI), rutting measurement system, downward scanning and automatic crack detection system and optional Ground Penetrating Radar (GPR) system.



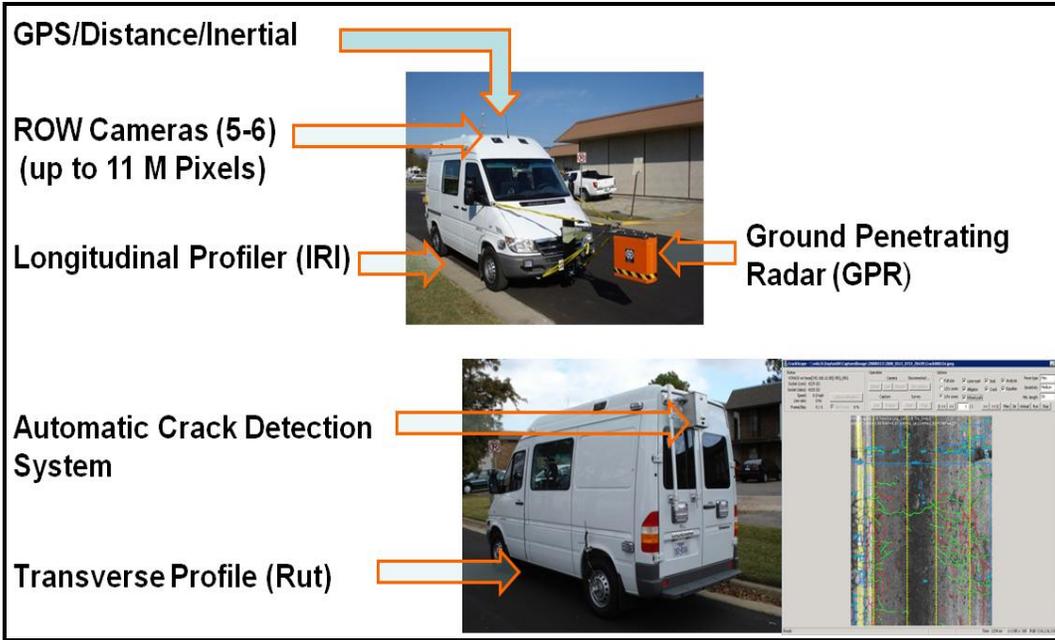


Figure 3-ERoadInfo Data Collection Vehicle

The ERoadInfo Playback and Asset Inventory module allows users to review images of the roadway and collect assets that appear in the field-of-view from the comfort of their desk.



Images from field Survey

Images

The images collected using the ERoadInfo roadway imaging module can be played back like a VCR. When reviewing the images, assets visible on the image can be

captured from the office desk by simply clicking on the location of the asset in the image.

The data is collected using the photogrammetry measurement based on the images' location and orientation information recorded during the survey. During this project this data was stored directly into a central GIS database and imported into Paver™ where the PCI was calculated.

4.0 DATA COLLECTION RESULTS

4.1 Pavement Inventory

In all, there were approximately 432 roadway segments based on the original GIS data from the City of Doral. Field survey indicated that some of these roadway segments were either grass surface or now private property and were removed from the PAVER database for the determination of the PCI. The total pavement area is approximately 7, 450 million square feet. Within the GIS data base, the City of Doral roads were classified as either class 4, 5 and 6. In Paver™, a section ranking was assigned to all pavement sections based on these classifications facilitate the data processing. Roads identified as class 4 were assigned a section ranking of "P" for primary, class 5 were assigned section ranking of "S" for secondary and class 6 assigned section ranking of "T" for tertiary. Approximately 68% of existing roadway within the City of Doral can be described as secondary, 17% as primary and 15% as tertiary. The breakdown of pavement area by section ranking is provided in figure 4. Additional information for each section including PCI, rank, surface type, and dimensions can be found in Appendix B.

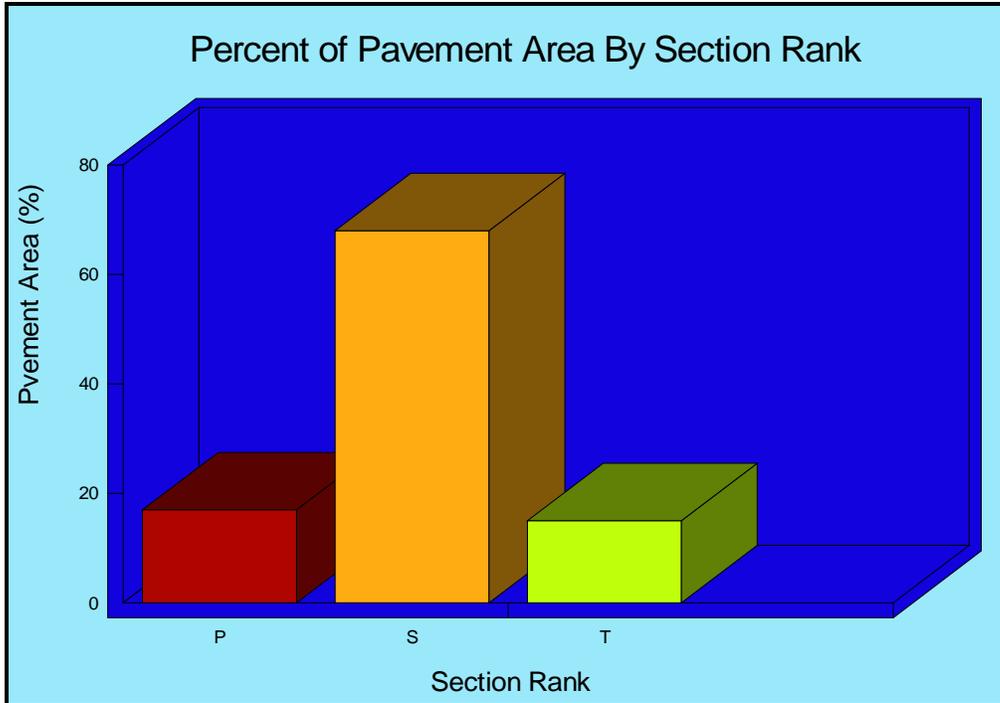


Figure 4-Pavement Area Distribution by Section Rank

4.1.1 History

Pavement condition work history is the most crucial information used by PAVER™ in computing PCI, determining deterioration curves and future maintenance plan. By knowing when pavement sections have been reconstructed or the last construction date, integrated with PAVER™, will allow the City of Doral to continue to successfully manage their roadway network now and in the future.

Most of the information obtained on the construction history was obtained from the 2007 PMS report conducted by Sanchez-Zeinali & Associates Inc. and their sub consultant Applied Research, Inc. (ARA). The report ultimately concluded that the information used was not very accurate. The City of Doral also provided additional information on a few pavement sections that were rehabilitated and have been included in the work history inventory in PAVER. Other pavement sections last



construction date was estimated by reviewing the Miami Dade County parcel records of developments adjacent to these pavement sections. Therefore, based on the limited information, the majority of roadway segments were treated as if they underwent major rehabilitation and reconstruction (R&R) characterized as “Complete Reconstruction AC” or “2-inch Cold Mill & Overlay” (milling and resurfacing). Figure 5 provides the pavement age at last inspection as a percentage of the total pavement that underwent major R&R. Nearly 75% of the pavement sections underwent major work between 6-15 years ago, while about 17% in the past 2 years.

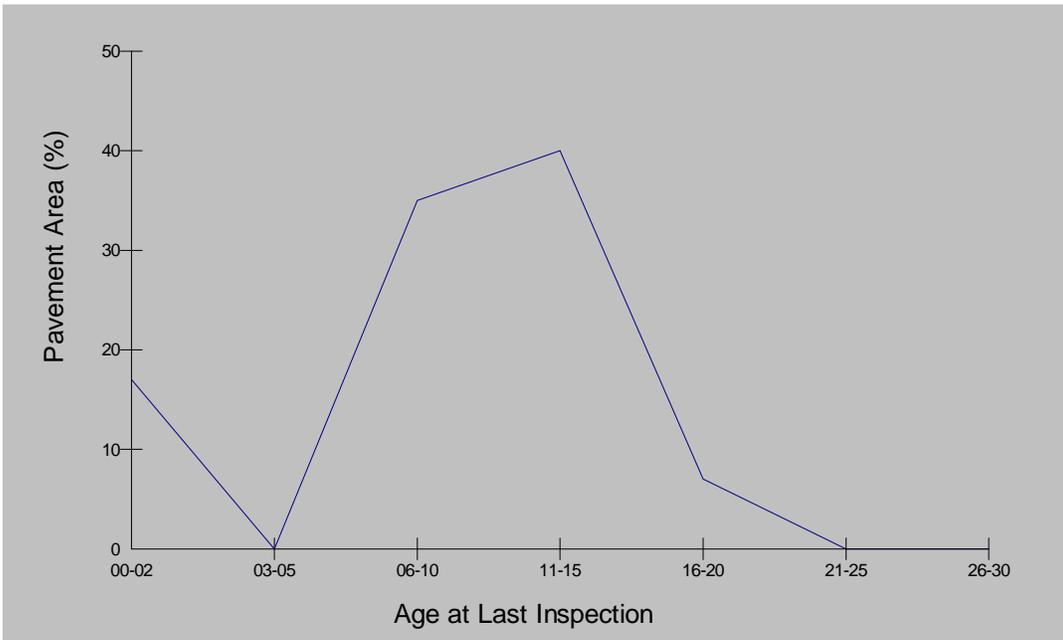


Figure 5-City of Doral Pavement Section Age Since Last Major R&R

A statistical summary of the section report is provided in table 2. The overall average age at inspection for the pavement network is



Table 2- Summary of Pavement Section Condition Report

Age Category	Average Age at Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	PCI Standard Deviation	Weighted Average PCI
0-02	0.95	1,293,818.43	87	98.11	5.01	97.77
03-05	5.00	5,157.63	1	100.00	0.00	100.00
06-10	8.49	2,587,564.48	162	85.30	14.56	85.93
11-15	12.97	2,986,957.83	146	84.25	11.98	84.85
16-20	16.40	532,547.89	30	81.87	12.32	82.39
All	9.04	7,406,046.26	426	87.35	13.29	87.32

4.2 Pavement Condition Index

Pavement survey was conducted in accordance with ASTM D 6433-09 which characterizes distress type, severity (Table 1) and quantity for sample areas within each pavement section. Visual data collected on pavement sections for distress, severity and quantities were imported into PAVER™ and the PCI value for each pavement section was calculated. The PCI returns a numerical rating value between 0 and 100 (Figure 2) to describe the condition of the pavement. A summary of pavement ratings for the City of Doral roadway network for 2011 as well as 2007 can be found in Appendix B.

4.2.1 PCI Results 2007 versus 2011

The PCI results between 2007 and 2011 shows for the majority of the roadway networks show an overall increase which is not consistent with the behavior of roadways. All pavements will deteriorate over time even if there is no traffic. In some segments the overall increase in PCI is expected since these roads underwent major (M&R) between 2009 and 2010. For example, NW 84th Avenue between NW 23rd Street and NW 25th Street had a PCI of rating of 44.9 in 2007 and 93 in 2011.



Information provided from the City of Doral indicates that this segment was milled and resurfaced during the fiscal year 2010-2011. By contrast, NW 52nd Street between NW 77th Ct and NW 78th Avenue has a PCI of 34.3 in 2007 and 73 in 2011. There is no record of the segment undergoing any M&R, but the increase in PCI suggests that some work must have been done on this pavement section. Some localized preventative or stop gap (filling potholes, patching etc.) maintenance must have been performed on this segment and others that show an increase in PCI for which no record of any M&R exist.

Figure 6 below provides the overall condition of the roadway network based on the percentage of pavement area. Overall approximately 63 percent of the pavement is considered in "Good" condition, 24 percent "Satisfactory" condition, 11 percent "Fair" condition, and 2 percent in "Poor" condition.

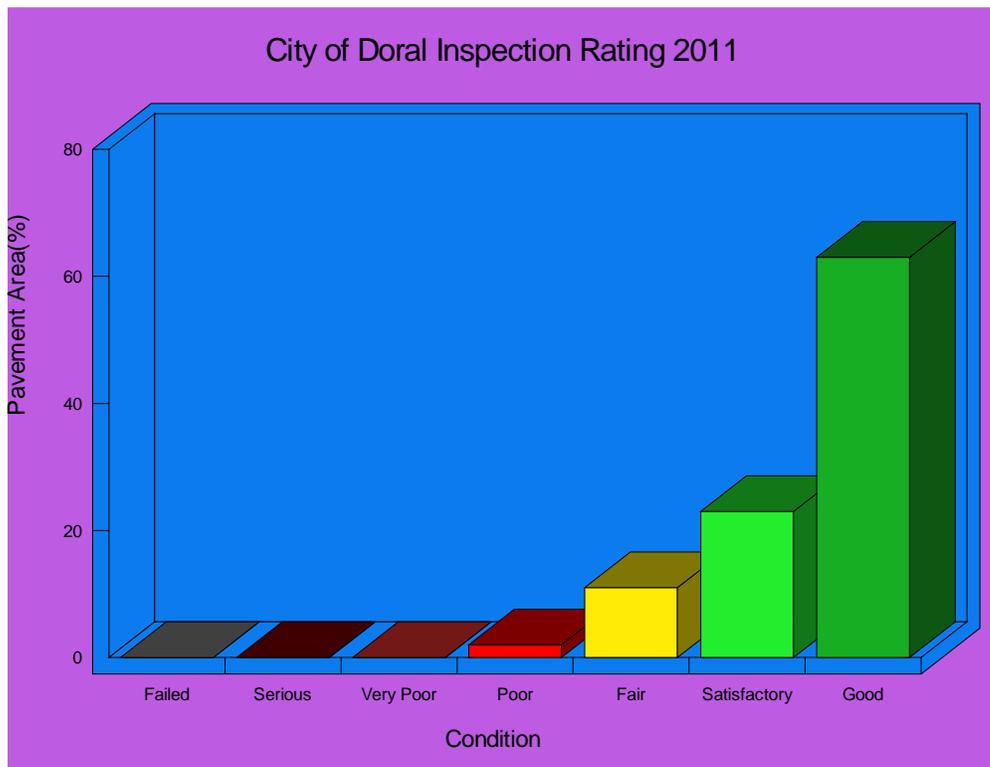


Figure 6-City of Doral Pavement Rating 2011

5.0 DATA CONDITION AND PREDICTION MODELS

5.1 Prediction/Deterioration Models

Paver™ uses what is called the Family Method to produce predication/deterioration models. Since PCI is used as an indicator of the overall pavement condition, the predication/deterioration models essentials looks at the PCI versus time in the future. The family method represents a group of pavement sections surveyed that have similar characteristic. In Paver™, the software allows users to select families based on last construction date, PCI, Rank, Surface, section category, type, Use, and Zones. Selections of these families are often driven based on available data and the overall accuracy of condition prediction. Before a successful family model can built we must first identify the pavement family, filter the data, conduct an outlier analysis, develop the model and finally predict the pavement section condition.

For this project, the pavement family used consisted of three primary categories used in Paver™ which included the branch use, surface type and section rank. In Paver™ the branch use is described as "Roadway", surface type as "asphalt-concrete (AC)" and the section rank as either "P", "S" or "T" as described in section 4.1 of this report. Three separates prediction models were developed based on branch, surface type and section rank. The branch and surface type were identical for each model but the section rank (P, S, or T) varied based on the individual rankings. Figure 7 shows the family prediction model for all secondary pavements ("S"). A review of the model equation and statistics show a 4th degree polynomial and a coefficient correlation 0.663 (1 is perfect correlation) and a standard deviation of 8.939. The further out the degree of polynomial, the less accurate the model becomes. A standard deviation under 12 is also an indication of a fairly reliable model.



Figure 7-Family Prediction Model "S"

The best fit curve will only extend out as far as the available data. An additional data point at year 25 and a PCI of 0 is added to force the curve to zero. This was done with all the other family models ("P &T") to enhance the pavement prediction curve using an estimated life of 25 years. The revised prediction model "S" and equation statistics is calculated and show in figure 8.

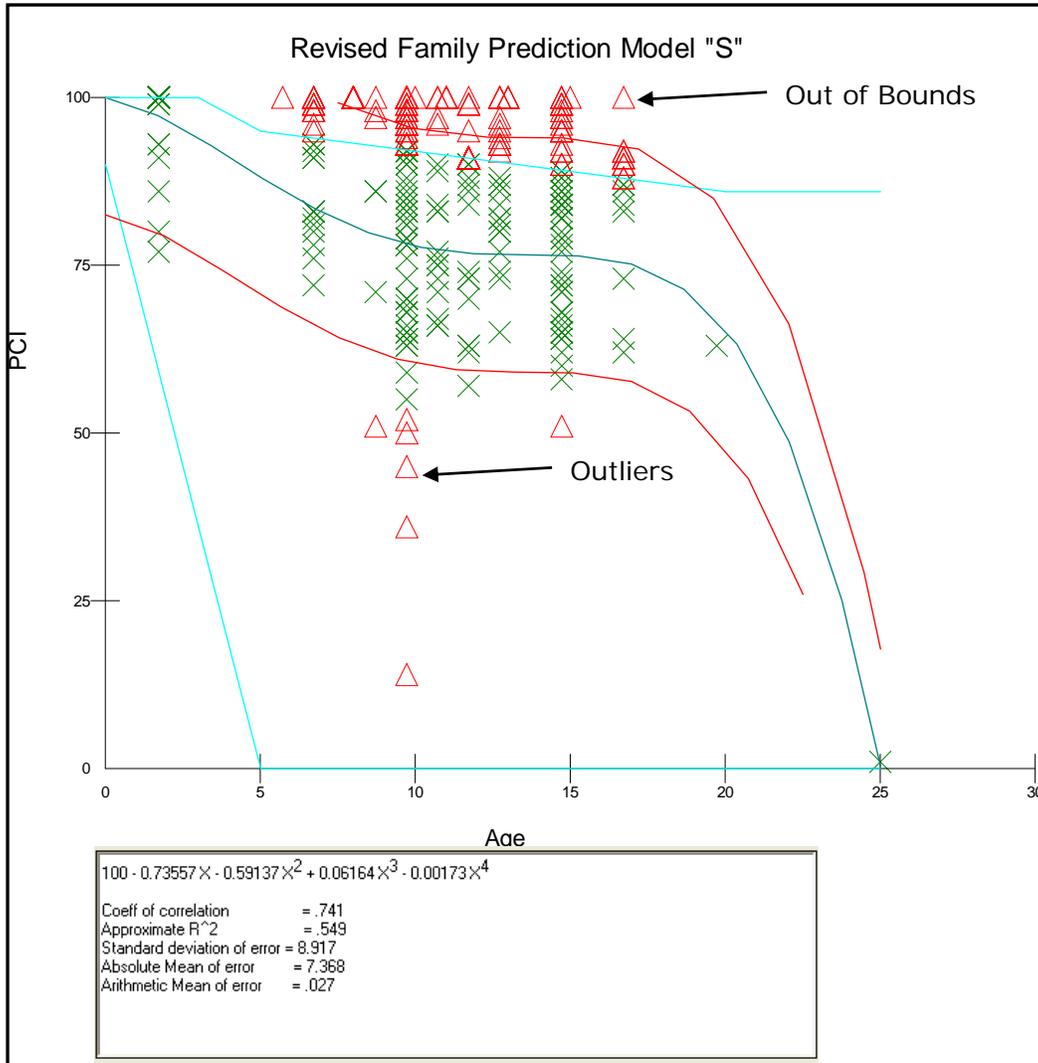


Figure 8-Revised Family Prediction Model "S"

In the revised family prediction model "S" (figure 8), there are several points which are classified as outliers or out of bounds and suggest there is an abnormality in these pavement segments that don't follow the same pattern as the other pavement sections. These outliers typically (PCIs lower than the predicted model) result from either a faulty record, inaccurate construction history or suggest that there is a serious problem in these pavement sections. The out of bounds points show a higher PCI value than the predicted model which indicate that the pavement has undergone major M&R prior to the last inspection date. Figure 9 shows a review of the model data which contains the PCI, the predicted model PCI and the status. A



review of the outlier's pavement sections in this model indicate a PCI below 55 which is where the Critical PCI is set. A review of the out of bounds pavement sections indicate that these pavement sections recently underwent major M&R based on the information received from the City of Doral. All secondary pavement sections will be assigned to this prediction model which will help developed M&R plans. Similar data section abnormalities were observed in family predication models "P" & "T".





PAVER 6.5.1

File Tables Preferences Add-Ins Window Help

Inventory Work PCI Reports Pred. Modeling Cond. Analysis M&R Plan GIS/Tree Set List Set Wizards Visual Menu Help About PAVER

Prediction Modeling

View

Age at Insp	PCI	Model	Difference	Status	NetworkID	BranchID	SectionID	Surface	Par
16.72	88.0	75.0	-13.0	Out of Bounds	CITYROAD	NW84THAVE	0070	AC	S
9.72	96.0	78.0	-18.0	Out of Bounds	CITYROAD	NW114THAVE	0270	AC	S
9.72	98.0	78.0	-20.0	Out of Bounds	CITYROAD	NW114THAVE	0080	AC	S
9.72	97.0	78.0	-19.0	Out of Bounds	CITYROAD	NW114THAVE	0110	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0310	AC	S
14.72	99.0	77.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0300	AC	S
16.72	92.0	75.0	-17.0	Out of Bounds	CITYROAD	NW84THAVE	0040	AC	S
16.72	90.0	75.0	-15.0	Out of Bounds	CITYROAD	NW84THAVE	0130	AC	S
12.73	93.0	77.0	-16.0	Out of Bounds	CITYROAD	NW52NDST	0050	AC	S
9.72	98.0	78.0	-20.0	Out of Bounds	CITYROAD	NW114THAVE	0320	AC	S
10.72	100.0	77.0	-23.0	Out of Bounds	CITYROAD	NW78THAVE	0020	AC	S
8.73	100.0	79.0	-21.0	Out of Bounds	CITYROAD	NW84THAVE	0060	AC	S
8.73	97.0	79.0	-18.0	Out of Bounds	CITYROAD	NW84THAVE	0110	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0060	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0070	AC	S
9.73	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0130	AC	S
11.72	99.0	77.0	-22.0	Out of Bounds	CITYROAD	NW113THCT	0010	AC	S
6.72	98.0	84.0	-14.0	Out of Bounds	CITYROAD	NW96THST	0040	AC	S
6.72	100.0	84.0	-16.0	Out of Bounds	CITYROAD	NW96THST	0020	AC	S
6.72	100.0	84.0	-16.0	Out of Bounds	CITYROAD	NW96THST	0060	AC	S
9.73	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0070	AC	S
11.0	100.0	77.0	-23.0	Out of Bounds	CITYROAD	NW99THAVE	0060	AC	S
12.73	96.0	77.0	-19.0	Out of Bounds	CITYROAD	NW52NDST	0040	AC	S
9.72	98.0	78.0	-20.0	Out of Bounds	CITYROAD	NW50THST	0050	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW50THST	0040	AC	S
8.0	100.0	81.0	-19.0	Out of Bounds	CITYROAD	NW98THAVE	0050	AC	S
8.0	100.0	81.0	-19.0	Out of Bounds	CITYROAD	NW98THAVE	0030	AC	S
9.72	97.0	78.0	-19.0	Out of Bounds	CITYROAD	NW50THST	0070	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW50THST	0020	AC	S
8.0	100.0	81.0	-19.0	Out of Bounds	CITYROAD	NW98THAVE	0020	AC	S
14.72	97.0	77.0	-20.0	Out of Bounds	CITYROAD	NW114THAVE	0030	AC	S
8.0	100.0	81.0	-19.0	Out of Bounds	CITYROAD	NW98THAVE	0040	AC	S
9.72	98.0	78.0	-20.0	Out of Bounds	CITYROAD	NW114THAVE	0190	AC	S
11.0	100.0	77.0	-23.0	Out of Bounds	CITYROAD	NW99THAVE	0080	AC	S
8.73	98.0	79.0	-19.0	Out of Bounds	CITYROAD	NW78THAVE	0040	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0180	AC	S
9.72	94.0	78.0	-16.0	Out of Bounds	CITYROAD	NW114THAVE	0200	AC	S
9.72	93.0	78.0	-15.0	Out of Bounds	CITYROAD	NW114THAVE	0170	AC	S
9.72	100.0	78.0	-22.0	Out of Bounds	CITYROAD	NW114THAVE	0120	AC	S
9.72	93.0	78.0	-15.0	Out of Bounds	CITYROAD	NW114THAVE	0260	AC	S
9.72	99.0	78.0	-21.0	Out of Bounds	CITYROAD	NW114THAVE	0010	AC	S
9.73	52.0	78.0	26.0	Outlier	CITYROAD	NW37THST	0010	AC	S
9.73	36.0	78.0	42.0	Outlier	CITYROAD	NW38THST	0010	AC	S
9.72	45.0	78.0	33.0	Outlier	CITYROAD	NW41STST	0080	AC	S
9.72	14.0	78.0	64.0	Outlier	CITYROAD	NW41STST	0010	AC	S
14.73	51.0	77.0	26.0	Outlier	CITYROAD	NW21STST	0050	AC	S
8.73	51.0	79.0	28.0	Outlier	CITYROAD	NW78THAVE	0080	AC	S
9.73	50.0	78.0	28.0	Outlier	CITYROAD	NW37THST	0020	AC	S

List Sele... Prediction ...

Figure 9-Review of Family Model Data "S"



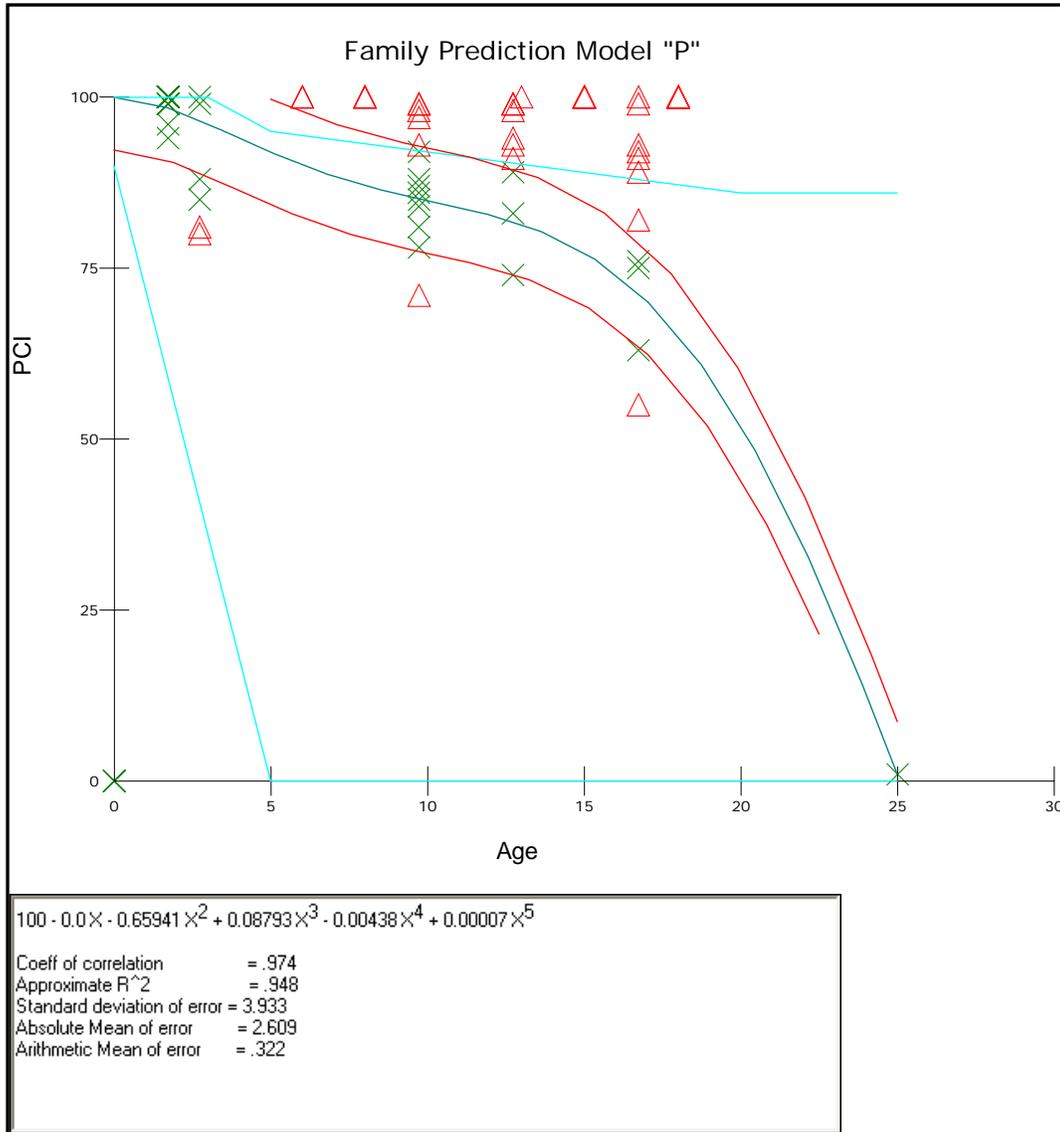


Figure 10-Review of Family Model Data "P"

Figure 10 shows the family prediction model for all the primary "P" pavement sections and the associated statistics. The model was manipulated in similar fashion as the family model "S" to enhance the best fit prediction curve.



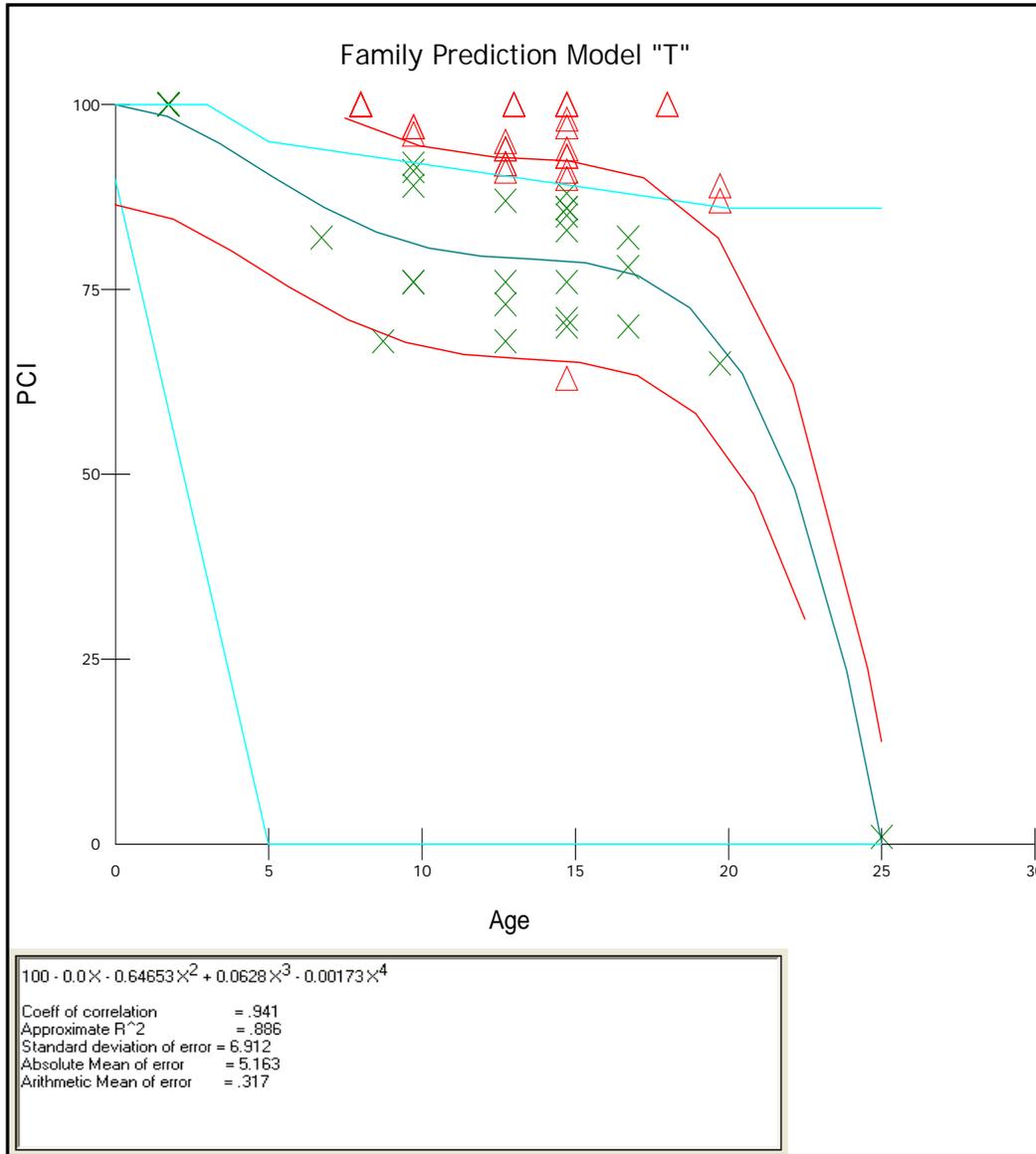


Figure 11-Review of Family Model Data "T"

Figure 11 shows the family prediction model for all the primary "T" pavement sections and the associated statistics. The model was manipulated in similar fashion as the family model "S" to enhance the best fit prediction curve.



Before the final M&R plan is implemented and accepted the outliers and out bounds pavement sections will have to be field verified to ensure there were no errors in the data collection.

5.2 Condition Analysis

The pavement condition analysis or condition forecast determines past and future performance of pavement networks or individual sections. Paver™ uses the last construction date and last inspection date which is then interpolated to determine past pavement condition. The “last construction date” was taken from the 2007 PMS report conducted by Sanchez-Zeinali & Associates Inc. which concluded that the dates were not very accurate due to limited information. Other information on “last construction date” was provided by the City of Doral and updated the corresponding pavement section. The future pavement condition is determined using the prediction/deterioration family models developed in 5.1. For each pavement rankings “P”, “S”, and “T”, three condition forecast models were developed as shown in Figures 12, 13 and 14 respectively. These models forecast the pavement predicted condition or PCI (Area weighted average) 10 years from the last inspection date (September 2011). Figures 12, 13 & 14 represent the rate of deterioration without an M&R plan implemented. These prediction condition models will be used to develop the M&R work plan which is discussed in section 6.0.

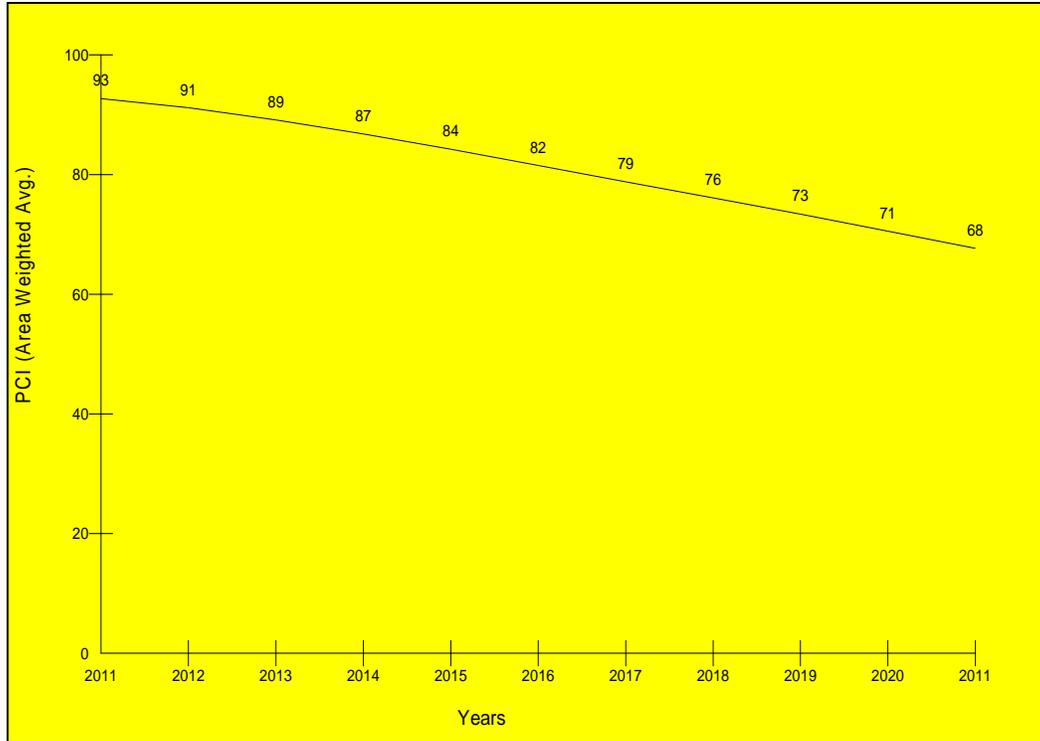


Figure 12-Predicted PCI for Primary "P" Pavement Sections

Figure 12 shows the forecast for the predicted PCI by area weighted average for primary pavement sections if there is no M&R intervention. At year 2011 the overall weighted PCI for primary pavement sections gradually decreases from 93 to around 68.



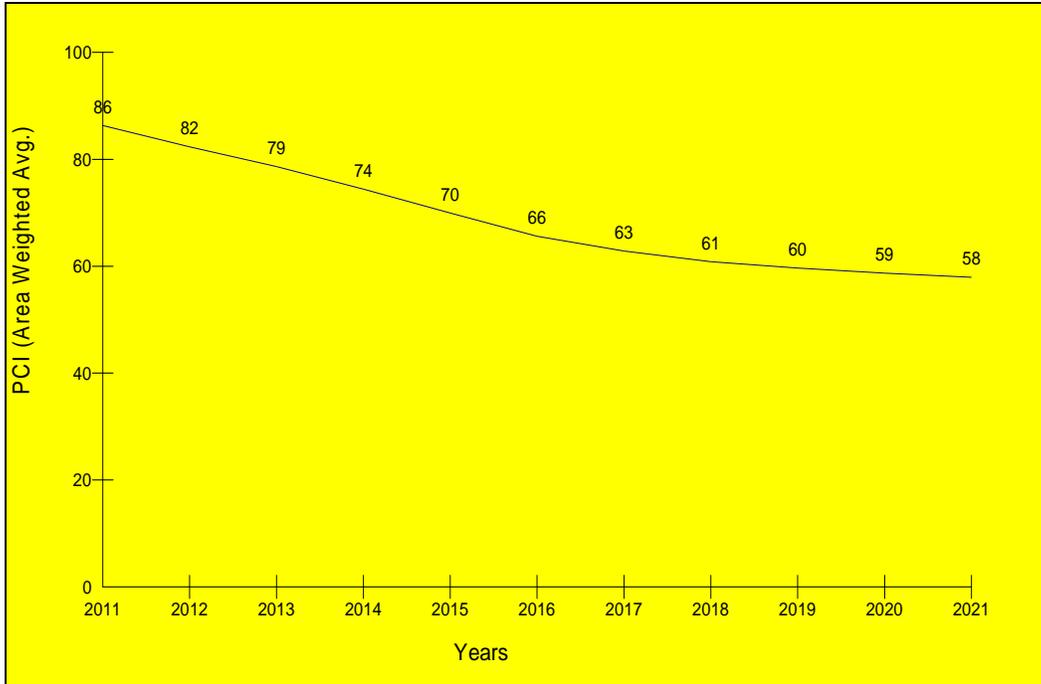


Figure 13-Predicted PCI for Secondary "S" Pavement Sections

Figure 13 shows the forecast for the predicted PCI by area weighted average for secondary pavement sections if there is no M&R intervention. At year 2011 the overall weighted PCI for secondary pavement sections gradually decreases from 86 to around 58.



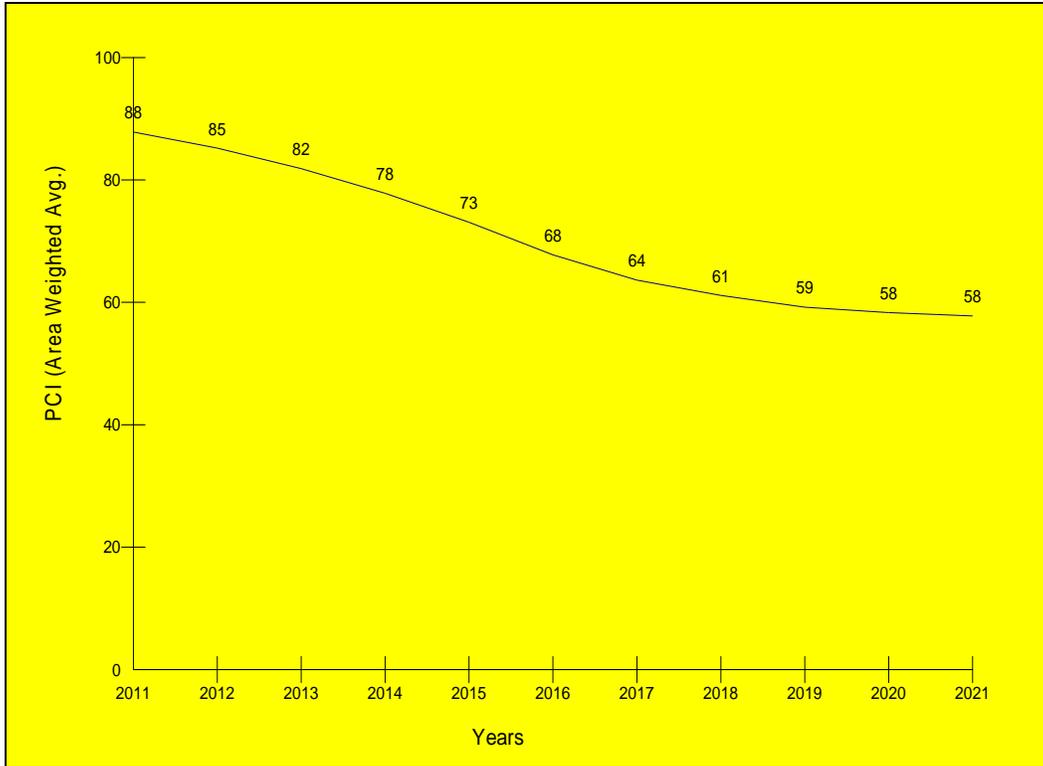


Figure 14-Predicted PCI for Tertiary "S" Pavement Sections

Figure 14 shows the forecast for the predicted PCI by area weighted average for secondary pavement sections if there is no M&R intervention. At year 2011 the overall weighted PCI for tertiary pavement sections gradually decreases from 88 to around 58.

6.0 MAINTAINANCE AND REHABILITATION POLICES AND UNIT COSTS

M&R Planning is a systematic approach where a set of activities are applied to pavement sections based on distresses within pavement, budgeting and condition indicators. Paver™ provides municipalities a variety of methods for M&R prioritization to roadway network or pavement sections. M&R activities for AC pavements are categorized in Paver™ as Localized Safety and Preventive, Global Preventive, and Major M&R.



Localized Stopgap (Safety) M&R policies are applied to various areas throughout the a pavement section whose PCI is below critical. This work is need to the keep pavement section operation and safe. This plan usually in involves filling of pot holes.

Localized Preventive M&R activities are applied to entire pavement sections whose PCI is above the critical PCI for the purpose of slowing pavement deterioration. This plan involves some surface treatment such as crack sealing and patching.

Global Preventive M&R activities are applied to entire pavement sections whose PCI is above the critical PCI for the purpose of slowing pavement deterioration. This plan involves some surface treatment placing a thin overlay. This option is typically not used in Florida.

Major M&R activities applied to the entire pavement section to improve existing structural requirements. It is used to upgrade pavements below the critical PCI or on pavements above critical PCI to maintain current condition.

C3TS recommends Major M&R for the City of Doral's AC pavement roadway network based on knowledge and professional judgment associated with PMS. Recommended work activities for M&R shall consist of complete pavement reconstruction, milling and resurfacing (1-inch), and 1-inch overlay. Given the amount of daily traffic Doral experiences from residence, business, and visitors, Local and Global treatment will not be adequate to maintain the current roadway network and meet the ever growing demand. Work plan and Budget analysis was performed for in Paver™ based on the Minimum PCI and Critical PCI.



6.1 M&R Planning Based on Minimum PCI

This approach establishes a work plan based on a selected or specified minimum PCI for a pavement network. When this option is selected, Paver™ provides an annual budget that is required to maintain all pavement levels above the minimum PCI (area weighted average) for the length of the plan. The family prediction models developed and discussed in section 5.1 are used to determine the year in which the PCI for each pavement section falls below this minimum PCI. When this occurs, Paver™ identifies these sections and multiplies the pavement section area by a set unit cost for M&R activity. Before this can be done, a PCI versus unit cost for major M&R must be established and entered into the system tables of the major M&R families in Paver™. "Pavement Management for Airports, Roads and Parking Lots 2005", Second Edition by M.Y. Shahin provides guidelines for establishing the relationship. Table 3 provides M&R activities and unit cost versus PCI used for this project.





Table 3-M & R Cost by Condition for Doral Roads

M&R Work	Condition	Cost	Unit
Reconstruction (8" Base, 2" AC)	0	\$6.50	SqFt
	10	\$6.50	SqFt
	20	\$6.50	SqFt
	30	\$6.50	SqFt
Milling and Resurfacing (1" AC)	40	\$2.00	SqFt
	50	\$2.00	SqFt
	60	\$2.00	SqFt
	70	\$2.00	SqFt
Overlay (1" AC)	80	\$1.50	SqFt
	90	\$1.50	SqFt
	100	\$1.50	SqFt

An inflation of 3 percent rate per year was applied to the M&R unit costs. These unit costs are based on historical estimates from the Florida Department of Transportation as well as similar projects managed by C3TS for various municipalities. The minimum PCI condition was then set at 86 for all City of Doral Roads over the length of work plan. This minimum value was established by recognizing the overall area weighted PCI for the predicted PCI curves for pavement rank "P", "S" and "T" in section 5.2 at the time of inspection was in good condition.

6.2 M&R Planning Based on Critical PCI Method

The Critical PCI is that point where a pavement begins to rapidly deteriorate with time. Paver™ uses this method to apply major M&R polices as well as Localized and Global which are not being considered. In typical asphalt pavement this critical PCI



ranges from 55 to 70. The Critical PCI allows municipalities to determine different budgeting scenarios when developing the M&R plan.

When a pavement section is above or equal to this critical PCI Paver™ verifies if the section is structurally deficient. If the pavement section is structurally deficient then major M&R is applied. If it is not then Localized M&R polices are assigned to the pavement section. Since these distresses are only known within the first year, PAVERTM uses a unit cost versus PCI to the cost in subsequent years (Table 3).

If a pavement section is below the Critical PCI the major M&R will be applied to the pavement section. If no funding is available then localized safety M&R is applied. Paver™ will apply Major M&R plan to all pavement sections that meet this criteria as long as funding is available.

6.3 M&R System Tables

A Cost by condition table (table 3) has been added in Paver™ for the purposes of this analysis. Paver™ M&R priority system tables by default sets the work priority for section rank as high for primary “P” pavement section, medium for secondary “S” pavement sections, and low for tertiary “T” pavement sections. For the purpose of this M&R plan a new M&R priority system tables was created and all pavement sections have been assigned a high priority ranking. In the Paver™ two new work types were added to the system work type tables. The first for a 1-inch Asphalt Concrete overlay (OL_1) and the other for 1-inch cold milling and overlay or milling and resurfacing existing AC for a depth on of one inch. One major disadvantage of this M&R plan is that major critical projects are given low priority.

7.0 REHABILITATION NEEDS AND BUDGET ANALYSIS

Paver™ was used to perform M&R analysis after customizing systems tables and establishing unit cost as discussed in section 6.0. An M&R plan was developed based on the minimum PCI being set to 86 for all pavement sections for a five year period. Figure 15 shows the annual amounts needed to maintain this minimum PCI with approximately \$5.5 million of the total \$8.6 million required in the first year. Figure 16 shows the average weighted PCI per year after work has been done.

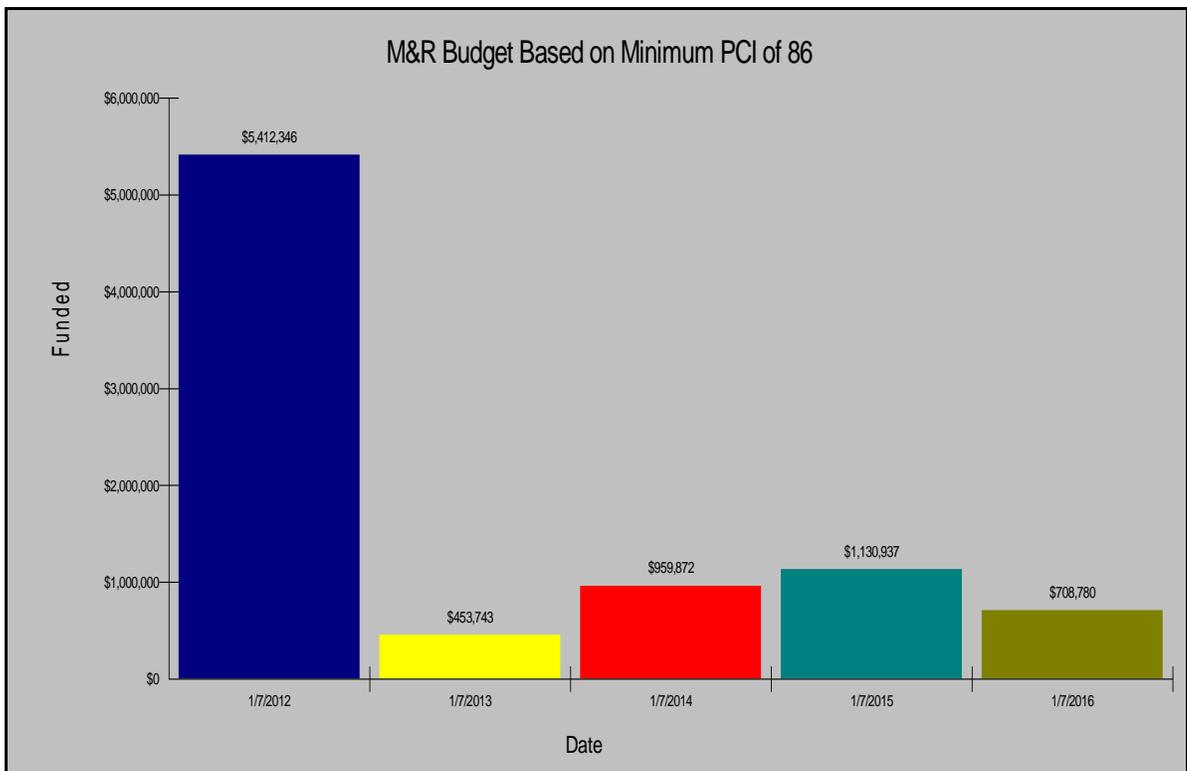


Figure 15-M&R Budget for Doral Based on Minimum PCI of 86

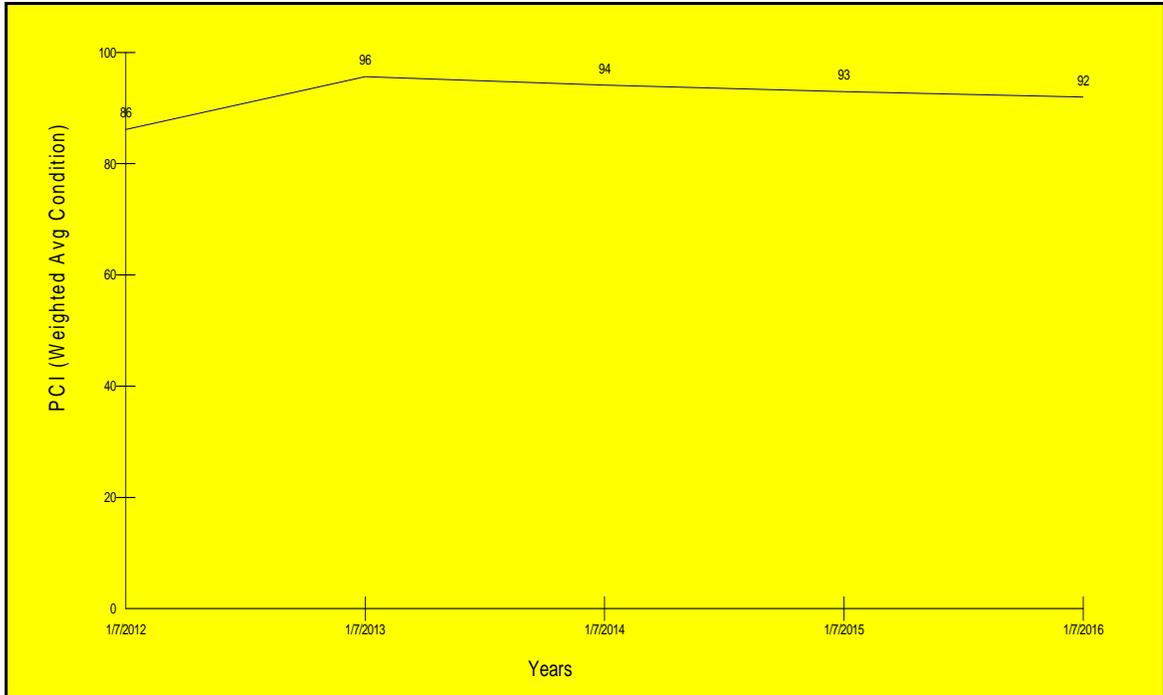


Figure 16-Average Weighted PCI After Repair Based on Minimum PCI of 86

The disadvantage of using the minimum PCI method is having such a high PCI in the first year calls for a high budget in the first year also. One alternative to reduce the initial cost vary the minimum PCI having a lower value in the initial years and maintaining 86 in the later years. Table 4 was then used to run the M&R analysis.

Table 4-Variable Minimum Conditional Table

Year	Minimum Condition
2012	80
2013	80
2014	80
2015	86
2016	86



Figure 17 shows the revised annual amounts needed to maintain the minimum PCI for each year in table 4. Approximately \$3.8 million of the total \$8.8 million will be required in the first year. Figure 18 shows the average weighted PCI per year after work has been done.

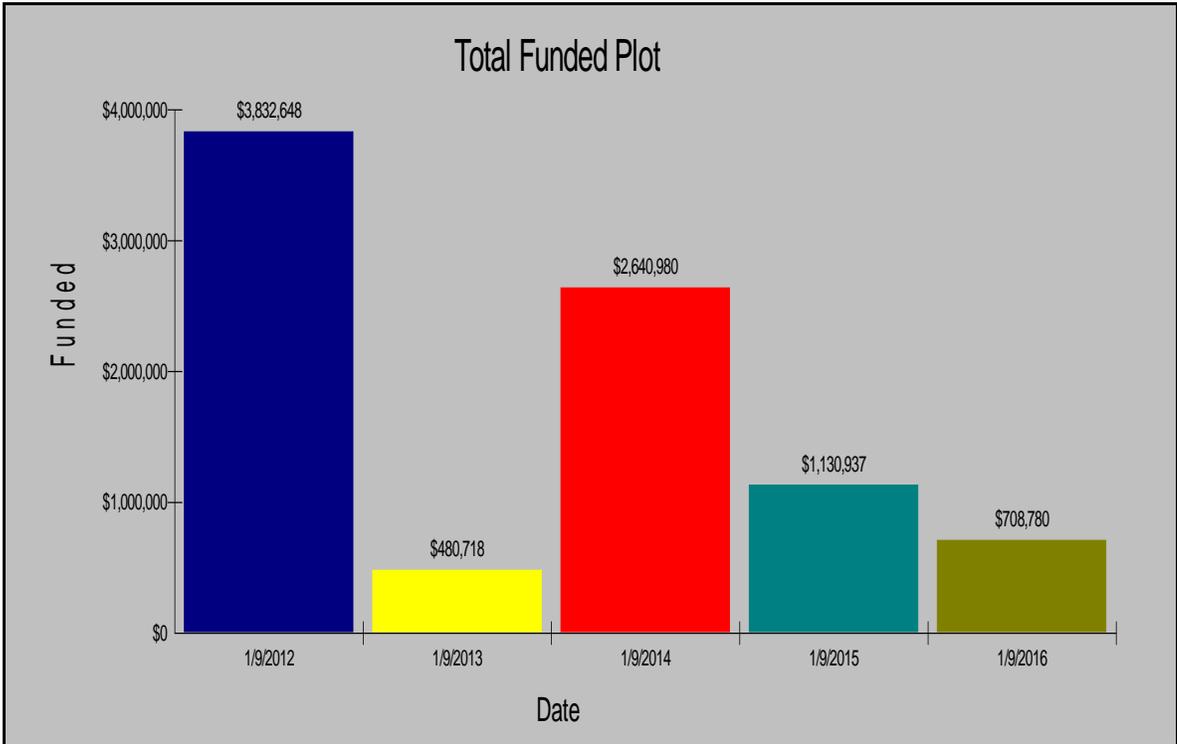


Figure 17-Revised M&R Budget for Doral Based on Minimum PCI of 86

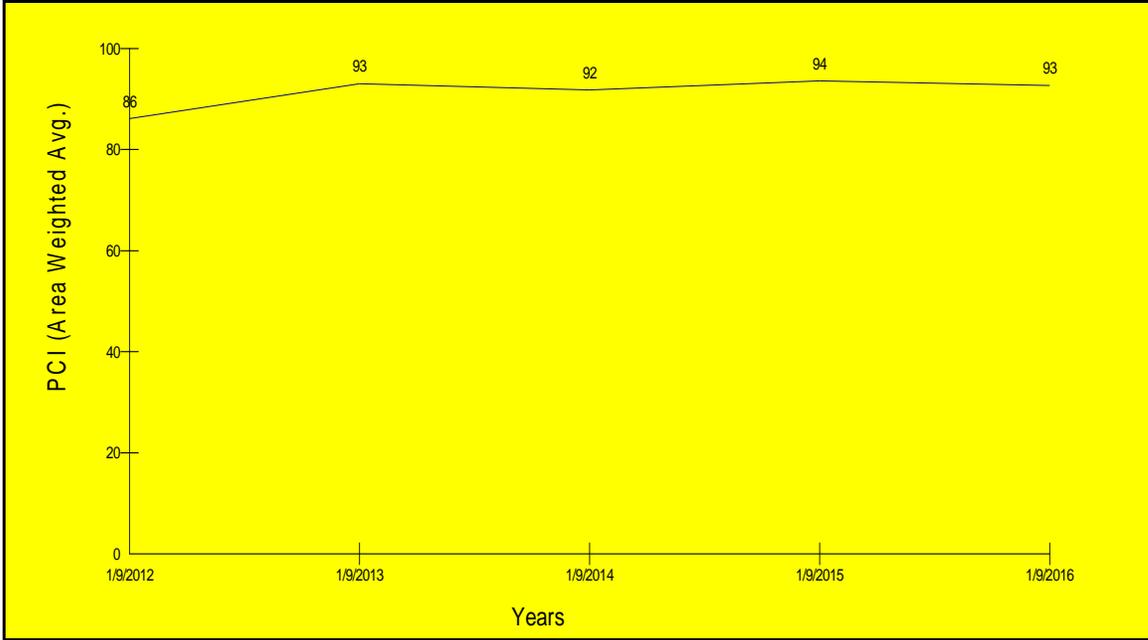


Figure 18- Revised Average Weighted PCI After Repair Based on Minimum PCI of 86

The revised M&R analysis (Minimum PCI based on Table 4) provides an overall weighted PCI above the minimum and reduces the annual cost compared to maintaining a constant PCI for the length of the term. Another disadvantage of this analysis is that it does not allow you to include any planned work which the City of Doral has expressed interest in performing. A work plan based on this M&R is provided in Appendix C.

Another M&R analysis was performed using the Critical PCI method. This method allows you to enter planned work as well as perform the required analysis and combined the budgets. In this method, the analysis was ran to maintain current area weighted PCI for the network with the critical PCI set to 70. No Localized or Global M&R work was assigned to any pavement sections in this analysis.

Figure 19 shows the revised annual amounts needed to maintain the current area weighted PCI for each year. Approximately \$1.9 million of the total \$5.2 million will

be required in the first year. Of that \$1.9 million, about \$ 1 million represents projects City of Doral requested added for the 2011/2012 fiscal year. See table 5 for budget summary. Figure 20 shows the average weighted PCI per year after work has been done.

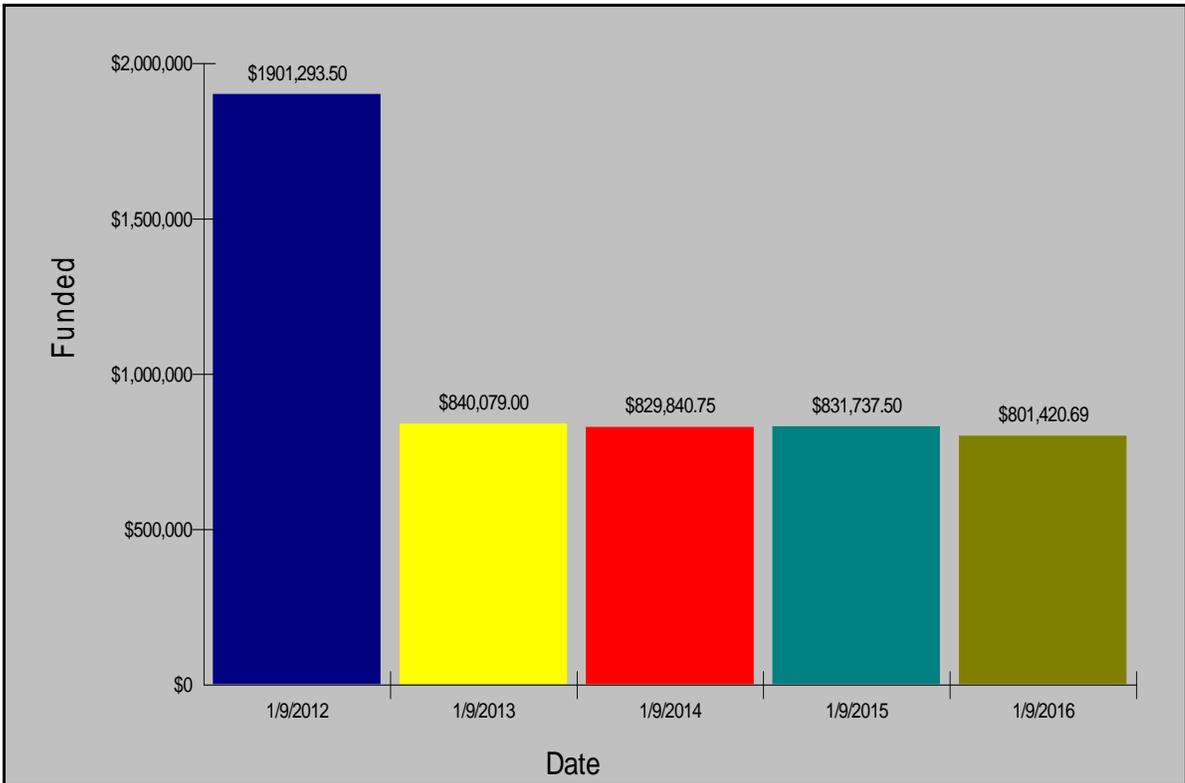


Figure 19-M&R Budget for Doral Based on Critical PCI Method

Table 5-Budget Summary for Critical PCI

Category	Total	2012	2013	2014	2015	2016	2017
Budget	\$5,043,619	\$840,603	\$840,603	\$840,603	\$840,603	\$840,603	\$840,603
Required Project(s)	\$1,062,294	\$1,062,294					
Work Planner	\$4,142,078	\$839,000	\$840,079	\$829,841	\$831,737	\$801,421	

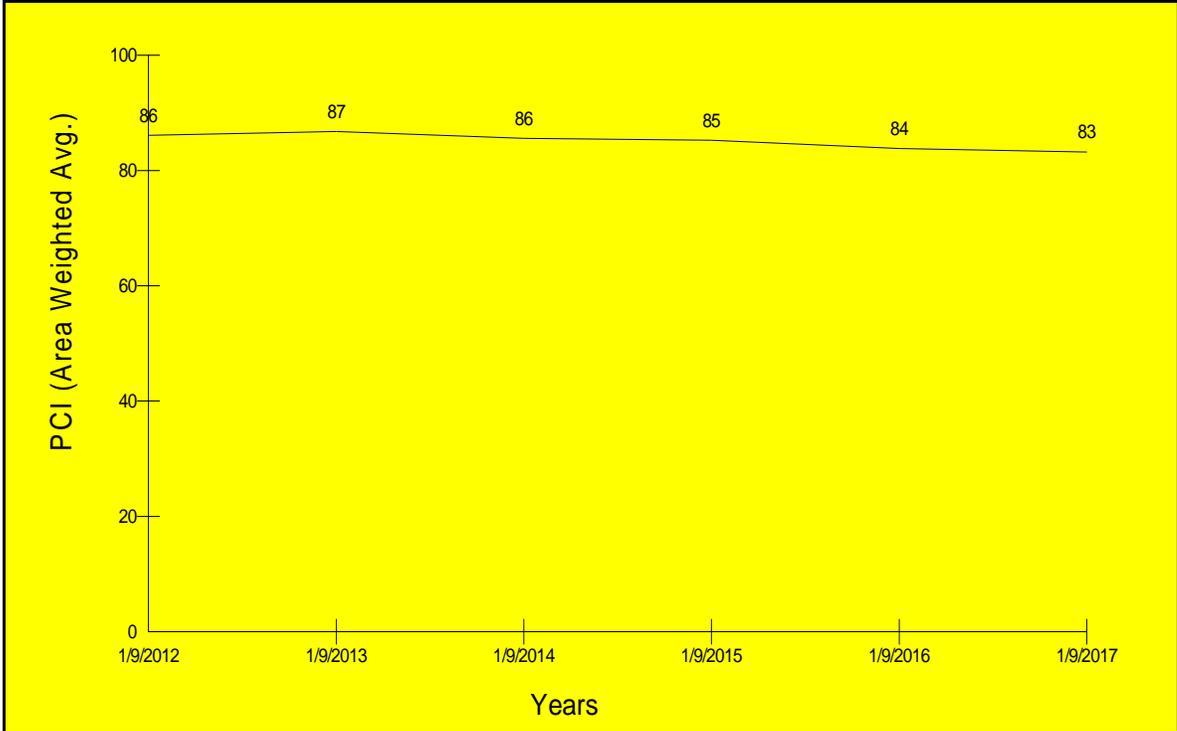


Figure 20-Average Weighted PCI After Repair Based on Critical PCI Method

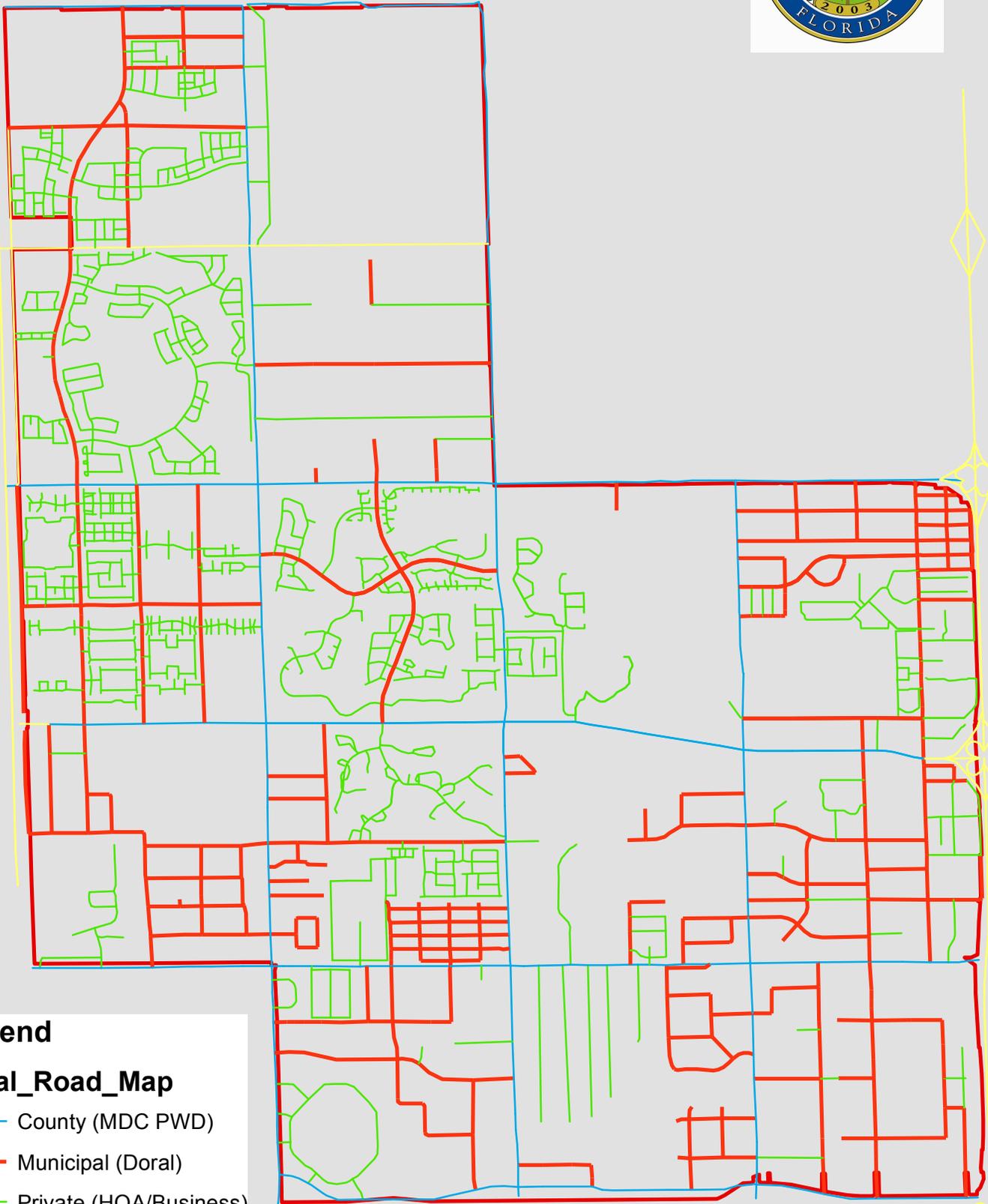


APPENDIX A

MAPS OF THE CITY OF DORAL



DORAL ROAD MAP 2011



Legend

Doral_Road_Map

- County (MDC PWD)
- Municipal (Doral)
- Private (HOA/Business)
- State (FDOT)



APPENDIX B

PAVEMENT CONDITION RESULTS



Section Condition Report

Pavement Database: DORAL_CLEAN_COPY NetworkID: CITYROAD

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW100THAVE (NW 100TH AVE)	0010	01/01/2002	AC	ROADWAY	S	2	5,347.07	09/22/2011	9	93.00
NW100THAVE (NW 100TH AVE)	0020	01/01/2002	AC	ROADWAY	S	2	4,740.61	09/22/2011	9	68.00
NW100THAVE (NW 100TH AVE)	0030	01/01/2002	AC	ROADWAY	S	2	4,729.38	09/22/2011	9	70.00
NW100THAVE (NW 100TH AVE)	0050	01/01/2000	AC	ROADWAY	S	2	19,680.63	09/22/2011	11	91.00
NW100THAVE (NW 100TH AVE)	0060	01/01/2002	AC	ROADWAY	S	2	4,570.95	09/22/2011	9	79.00
NW102NDAVE (NW 102ND AVE)	0070	01/01/2002	AC	ROADWAY	P	2	12,160.85	09/23/2011	9	71.00
NW102NDAVE (NW 102ND AVE)	0090	01/01/2000	AC	ROADWAY	S	2	26,223.95	09/22/2011	11	95.00
NW102NDAVE (NW 102ND AVE)	0100	01/01/2002	AC	ROADWAY	S	2	26,913.31	09/23/2011	9	97.00
NW102NDAVE (NW 102ND AVE)	0110	01/01/2002	AC	ROADWAY	S	2	6,110.83	09/22/2011	9	68.00
NW102NDAVE (NW 102ND AVE)	0120	01/01/2010	AC	ROADWAY	S	2	36,791.56	09/22/2011	1	100.00
NW102NDAVE (NW 102ND AVE)	0130	01/01/2000	AC	ROADWAY	S	2	5,818.02	09/22/2011	11	57.00
NW102NDAVE (NW 102ND AVE)	0140	01/01/2000	AC	ROADWAY	S	2	2,345.00	09/22/2011	11	73.00
NW102NDAVE (NW 102ND AVE)	0150	01/01/2000	AC	ROADWAY	S	2	5,158.75	09/22/2011	11	73.00
NW102NDAVE (NW 102ND AVE)	0160	01/01/2000	AC	ROADWAY	S	2	5,821.80	09/22/2011	11	74.00
NW102NDAVE (NW 102ND AVE)	0170	01/01/2002	AC	ROADWAY	P	2	5,548.59	09/23/2011	9	97.00
NW102NDAVE (NW 102ND AVE)	0180	01/01/2000	AC	ROADWAY	S	2	25,160.96	09/22/2011	11	99.00
NW102NDAVE (NW 102ND AVE)	0190	01/01/2002	AC	ROADWAY	P	2	14,722.80	09/23/2011	9	78.00
NW102NDAVE (NW 102ND AVE)	0200	01/01/2002	AC	ROADWAY	P	2	3,656.08	09/23/2011	9	85.00
NW102NDAVE (NW 102ND AVE)	0210	01/01/2002	AC	ROADWAY	P	2	22,269.56	09/23/2011	9	88.00
NW102NDAVE (NW 102ND AVE)	0240	01/01/2002	AC	ROADWAY	S	2	24,097.55	01/01/2002	0	100.00
NW102NDAVE (NW 102ND AVE)	0250	01/01/2002	AC	ROADWAY	S	2	1,910.79	09/22/2011	9	96.00
NW102NDAVE (NW 102ND AVE)	0260	01/01/2002	AC	ROADWAY	P	2	38,921.39	09/23/2011	9	92.00
NW102NDAVE (NW 102ND AVE)	0270	01/01/2002	AC	ROADWAY	P	2	10,774.42	09/23/2011	9	99.00
NW102NDAVE (NW 102ND AVE)	0280	01/01/2002	AC	ROADWAY	P	2	11,838.68	09/23/2011	9	99.00
NW102NDAVE (NW 102ND AVE)	0290	01/01/2002	AC	ROADWAY	P	2	27,520.50	09/23/2011	9	99.00
NW102NDPL (NW 102ND PL)	0010	01/01/1999	AC	ROADWAY	S	2	36,692.04	09/22/2011	12	82.00
NW104THAVE (NW 104TH AVE)	0010	01/01/2001	AC	ROADWAY	S	2	11,006.64	09/23/2011	10	76.00

Section Condition Report

Pavement Database: DORAL_CLEAN_COPY NetworkID: CITYROAD

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW104THAVE (NW 104TH AVE)	0020	01/01/2001	AC	ROADWAY	S	2	25,518.04	09/23/2011	10	83.00
NW104THAVE (NW 104TH AVE)	0030	01/01/2001	AC	ROADWAY	S	2	6,720.74	09/24/2011	10	71.00
NW104THAVE (NW 104TH AVE)	0040	01/01/2001	AC	ROADWAY	S	2	21,445.59	09/23/2011	10	96.00
NW104THCT (NW 104TH CT)	0010	01/01/2002	AC	ROADWAY	T	2	19,376.25	09/22/2011	9	92.00
NW105THAVE (NW 105TH AVE)	0010	01/01/2002	AC	ROADWAY	S	2	12,581.85	09/23/2011	9	92.00
NW105THAVE (NW 105TH AVE)	0020	01/01/2002	AC	ROADWAY	T	2	8,888.48	09/22/2011	9	96.00
NW105THAVE (NW 105TH AVE)	0030	01/01/2002	AC	ROADWAY	T	2	10,305.77	09/22/2011	9	97.00
NW108THAVE (NW 108TH AVE)	0010	01/01/1997	AC	ROADWAY	T	2	19,291.78	09/22/2011	14	100.00
NW108THAVE (NW 108TH AVE)	0020	01/01/1997	AC	ROADWAY	T	2	19,231.96	09/22/2011	14	88.00
NW108THAVE (NW 108TH AVE)	0030	01/01/1997	AC	ROADWAY	T	2	20,580.59	09/22/2011	14	100.00
NW109THAVE (NW 109TH AVE)	0010	01/01/2005	AC	ROADWAY	S	2	27,237.12	09/23/2011	6	83.00
NW109THAVE (NW 109TH AVE)	0020	01/01/2005	AC	ROADWAY	S	2	28,153.48	09/23/2011	6	93.00
NW109THAVE (NW 109TH AVE)	0030	01/01/2005	AC	ROADWAY	S	2	18,020.14	09/23/2011	6	98.00
NW109THAVE (NW 109TH AVE)	0040	01/01/2005	AC	ROADWAY	S	2	8,756.26	09/23/2011	6	98.00
NW109THAVE (NW 109TH AVE)	0050	01/01/2010	AC	ROADWAY	S	2	21,129.38	09/22/2011	1	100.00
NW109THAVE (NW 109TH AVE)	0060	01/01/2010	AC	ROADWAY	S	2	7,285.80	09/22/2011	1	100.00
NW109THAVE (NW 109TH AVE)	0070	01/01/2005	AC	ROADWAY	S	2	31,547.87	09/23/2011	6	76.00
NW109THAVE (NW 109TH AVE)	0080	01/01/2010	AC	ROADWAY	S	2	18,877.06	09/22/2011	1	100.00
NW109THAVE (NW 109TH AVE)	0090	01/01/2010	AC	ROADWAY	S	2	11,259.67	09/22/2011	1	100.00
NW109THAVE (NW 109TH AVE)	0100	01/01/2005	AC	ROADWAY	S	2	8,972.77	09/23/2011	6	96.00
NW109THAVE (NW 109TH AVE)	0110	01/01/2005	AC	ROADWAY	S	2	8,859.04	09/23/2011	6	98.00
NW109THAVE (NW 109TH AVE)	0120	01/01/2001	AC	ROADWAY	S	2	18,604.34	09/23/2011	10	97.00
NW109THAVE (NW 109TH AVE)	0130	01/01/2005	AC	ROADWAY	S	2	9,918.07	09/23/2011	6	99.00
NW110THAVE (NW 110TH AVE)	0010	01/01/2002	AC	ROADWAY	S	2	4,538.72	09/22/2011	9	85.00
NW112THAVE (NW 112TH AVE)	0010	01/01/2005	AC	ROADWAY	S	4	9,071.88	09/23/2011	6	91.00
NW112THAVE (NW 112TH AVE)	0020	01/01/2002	AC	ROADWAY	S	2	41,389.06	09/23/2011	9	96.00

Section Condition Report

Pavement Database: DORAL_CLEAN_COPY NetworkID: CITYROAD

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW112THAVE (NW 112TH AVE)	0030	01/01/2010	AC	ROADWAY	T	2	20,861.19	09/22/2011	1	100.00
NW112THAVE (NW 112TH AVE)	0040	01/01/2005	AC	ROADWAY	S	2	5,746.88	09/23/2011	6	91.00
NW112THAVE (NW 112TH AVE)	0050	01/01/2010	AC	ROADWAY	T	2	19,397.29	09/22/2011	1	100.00
NW112THAVE (NW 112TH AVE)	0060	01/01/2005	AC	ROADWAY	S	2	7,981.34	09/23/2011	6	82.00
NW112THAVE (NW 112TH AVE)	0070	01/01/2005	AC	ROADWAY	S	2	5,053.04	09/23/2011	6	78.00
NW112THAVE (NW 112TH AVE)	0080	01/01/2005	AC	ROADWAY	S	2	13,405.05	09/23/2011	6	83.00
NW112THAVE (NW 112TH AVE)	0090	01/01/2002	AC	ROADWAY	S	2	21,183.87	09/23/2011	9	99.00
NW112THAVE (NW 112TH AVE)	0100	01/01/2005	AC	ROADWAY	S	2	5,499.75	09/23/2011	6	81.00
NW112THAVE (NW 112TH AVE)	0110	01/01/2002	AC	ROADWAY	S	2	25,050.42	09/23/2011	9	100.00
NW112THAVE (NW 112TH AVE)	0120	01/01/2002	AC	ROADWAY	S	2	17,935.93	09/23/2011	9	93.00
NW112THAVE (NW 112TH AVE)	0130	01/01/2005	AC	ROADWAY	S	2	8,219.23	09/23/2011	6	95.00
NW112THAVE (NW 112TH AVE)	0140	01/01/2005	AC	ROADWAY	S	2	4,737.38	09/23/2011	6	83.00
NW112THAVE (NW 112TH AVE)	0150	01/01/2002	AC	ROADWAY	S	2	6,340.44	09/23/2011	9	98.00
NW112THAVE (NW 112TH AVE)	0160	01/01/2002	AC	ROADWAY	S	2	19,394.56	09/23/2011	9	96.00
NW112THAVE (NW 112TH AVE)	0170	01/01/2005	AC	ROADWAY	S	2	11,654.98	09/23/2011	6	100.00
NW112THAVE (NW 112TH AVE)	0180	01/01/2005	AC	ROADWAY	S	2	7,829.70	09/23/2011	6	99.00
NW112THAVE (NW 112TH AVE)	0190	01/01/2010	AC	ROADWAY	S	2	18,817.81	09/22/2011	1	100.00
NW112THAVE (NW 112TH AVE)	0200	01/01/2005	AC	ROADWAY	S	2	9,715.19	09/23/2011	6	100.00
NW112THAVE (NW 112TH AVE)	0210	01/01/2005	AC	ROADWAY	S	2	4,807.04	09/23/2011	6	72.00
NW112THAVE (NW 112TH AVE)	0220	01/01/2005	AC	ROADWAY	S	2	5,140.14	09/23/2011	6	93.00
NW112THAVE (NW 112TH AVE)	0230	01/01/2002	AC	ROADWAY	S	2	19,895.47	09/23/2011	9	95.00
NW112THAVE (NW 112TH AVE)	0240	01/01/2002	AC	ROADWAY	S	2	13,439.38	09/23/2011	9	91.00
NW112THAVE (NW 112TH AVE)	0250	01/01/2005	AC	ROADWAY	S	2	5,141.81	09/23/2011	6	100.00
NW112THAVE (NW 112TH AVE)	0260	01/01/2010	AC	ROADWAY	S	2	21,414.87	09/22/2011	1	100.00
NW112THAVE (NW 112TH AVE)	0270	01/01/2010	AC	ROADWAY	S	2	9,151.38	09/22/2011	1	77.00
NW112THAVE (NW 112TH AVE)	0280	01/01/2005	AC	ROADWAY	S	2	25,963.06	09/23/2011	6	99.00
NW113THCT (NW 113TH CT)	0010	01/01/2000	AC	ROADWAY	S	2	25,384.53	09/22/2011	11	99.00

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW114THAVE (NW 114TH AVE)	0010	01/01/2002	AC	ROADWAY	S	2	4,168.39	09/24/2011	9	59.00
NW114THAVE (NW 114TH AVE)	0020	01/01/2002	AC	ROADWAY	S	2	8,925.89	09/24/2011	9	65.00
NW114THAVE (NW 114TH AVE)	0030	01/01/1997	AC	ROADWAY	S	2	12,018.47	09/24/2011	14	65.00
NW114THAVE (NW 114TH AVE)	0040	01/01/2002	AC	ROADWAY	S	2	11,430.41	09/24/2011	9	65.00
NW114THAVE (NW 114TH AVE)	0050	01/01/2002	AC	ROADWAY	S	2	25,358.56	09/23/2011	9	98.00
NW114THAVE (NW 114TH AVE)	0060	01/01/2002	AC	ROADWAY	S	2	19,567.52	09/24/2011	9	100.00
NW114THAVE (NW 114TH AVE)	0070	01/01/2002	AC	ROADWAY	S	2	7,998.06	09/24/2011	9	100.00
NW114THAVE (NW 114TH AVE)	0080	01/01/2002	AC	ROADWAY	S	2	7,968.07	09/23/2011	9	98.00
NW114THAVE (NW 114TH AVE)	0090	01/01/2002	AC	ROADWAY	S	2	18,876.24	09/24/2011	9	92.00
NW114THAVE (NW 114TH AVE)	0100	01/01/2002	AC	ROADWAY	S	2	8,902.85	09/23/2011	9	88.00
NW114THAVE (NW 114TH AVE)	0110	01/01/2002	AC	ROADWAY	S	2	27,464.31	09/23/2011	9	97.00
NW114THAVE (NW 114TH AVE)	0120	01/01/2002	AC	ROADWAY	S	2	15,358.06	09/24/2011	9	70.00
NW114THAVE (NW 114TH AVE)	0130	01/01/2002	AC	ROADWAY	S	2	11,873.54	09/24/2011	9	100.00
NW114THAVE (NW 114TH AVE)	0140	01/01/1997	AC	ROADWAY	S	2	14,878.65	09/24/2011	14	65.00
NW114THAVE (NW 114TH AVE)	0150	01/01/2002	AC	ROADWAY	S	2	8,364.74	09/24/2011	9	70.00
NW114THAVE (NW 114TH AVE)	0160	01/01/2002	AC	ROADWAY	S	2	12,431.92	09/24/2011	9	76.00
NW114THAVE (NW 114TH AVE)	0170	01/01/2002	AC	ROADWAY	S	2	6,658.84	09/24/2011	9	83.00
NW114THAVE (NW 114TH AVE)	0180	01/01/2002	AC	ROADWAY	S	2	11,209.65	09/24/2011	9	82.00
NW114THAVE (NW 114TH AVE)	0190	01/01/2002	AC	ROADWAY	S	2	19,282.46	09/23/2011	9	98.00
NW114THAVE (NW 114TH AVE)	0200	01/01/2002	AC	ROADWAY	S	2	15,947.39	09/23/2011	9	94.00
NW114THAVE (NW 114TH AVE)	0210	01/01/2002	AC	ROADWAY	S	2	13,833.09	09/24/2011	9	63.00
NW114THAVE (NW 114TH AVE)	0220	01/01/2002	AC	ROADWAY	S	2	2,851.41	01/01/2002	0	100.00
NW114THAVE (NW 114TH AVE)	0230	01/01/2002	AC	ROADWAY	S	2	11,701.74	09/24/2011	9	64.00
NW114THAVE (NW 114TH AVE)	0240	01/01/2002	AC	ROADWAY	S	2	8,831.67	09/24/2011	9	65.00
NW114THAVE (NW 114TH AVE)	0250	01/01/2002	AC	ROADWAY	S	2	18,815.82	09/23/2011	9	100.00
NW114THAVE (NW 114TH AVE)	0260	01/01/2002	AC	ROADWAY	S	2	6,588.00	09/23/2011	9	93.00
NW114THAVE (NW 114TH AVE)	0270	01/01/2002	AC	ROADWAY	S	2	27,826.49	09/23/2011	9	96.00

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW114THAVE (NW 114TH AVE)	0280	01/01/1997	AC	ROADWAY	S	2	15,298.25	09/24/2011	14	79.00
NW114THAVE (NW 114TH AVE)	0300	01/01/1997	AC	ROADWAY	S	2	18,547.83	09/24/2011	14	68.00
NW114THAVE (NW 114TH AVE)	0310	01/01/2002	AC	ROADWAY	S	2	11,948.41	09/24/2011	9	69.00
NW114THAVE (NW 114TH AVE)	0320	01/01/2002	AC	ROADWAY	S	2	11,797.36	09/23/2011	9	98.00
NW115THAVE (NW 115TH AVE)	0010	01/01/1997	AC	ROADWAY	S	2	52,655.43	09/22/2011	14	96.00
NW115THAVE (NW 115TH AVE)	0020	01/01/1997	AC	ROADWAY	S	2	19,506.53	09/22/2011	14	97.00
NW117THAVE (NW 117TH AVE)	0010	01/01/2000	AC	ROADWAY	S	2	47,359.22	09/23/2011	11	100.00
NW117THAVE (NW 117TH AVE)	0020	01/01/2000	AC	ROADWAY	S	2	38,183.83	09/23/2011	11	63.00
NW117THAVE (NW 117TH AVE)	0030	01/01/2000	AC	ROADWAY	S	2	15,094.70	09/23/2011	11	84.00
NW117THAVE (NW 117TH AVE)	0040	01/01/2000	AC	ROADWAY	S	2	54,820.80	09/22/2011	11	63.00
NW13THST (NW 13TH ST)	0010	01/01/2010	AC	ROADWAY	T	2	5,775.02	09/22/2011	1	100.00
NW13THST (NW 13TH ST)	0020	01/01/2010	AC	ROADWAY	T	2	10,436.42	09/22/2011	1	100.00
NW13THST (NW 13TH ST)	0030	01/01/2010	AC	ROADWAY	T	2	19,630.02	09/22/2011	1	100.00
NW13THST (NW 13TH ST)	0040	01/01/2010	AC	ROADWAY	T	2	12,731.33	09/22/2011	1	100.00
NW13THTER (NW 13TH TER)	0010	01/01/2002	AC	ROADWAY	T	2	6,156.88	09/22/2011	9	89.00
NW13THTER (NW 13TH TER)	0020	01/01/2002	AC	ROADWAY	T	2	22,573.12	09/22/2011	9	76.00
NW13THTER (NW 13TH TER)	0030	01/01/2002	AC	ROADWAY	T	2	15,911.49	09/22/2011	9	76.00
NW14THST (NW 14TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	32,483.67	09/22/2011	14	99.00
NW14THST (NW 14TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	22,757.05	09/24/2011	14	85.00
NW14THST (NW 14TH ST)	0030	01/01/1997	AC	ROADWAY	S	2	46,442.96	09/24/2011	14	84.00
NW15THST (NW 15TH ST)	0010	01/01/1992	AC	ROADWAY	S	2	12,618.75	09/24/2011	19	63.00
NW15THST (NW 15TH ST)	0020	01/01/2005	AC	ROADWAY	T	2	5,729.65	09/22/2011	6	82.00
NW15THST (NW 15TH ST)	0030	01/01/1999	AC	ROADWAY	T	2	16,373.50	09/22/2011	12	76.00
NW15THST (NW 15TH ST)	0040	01/01/1999	AC	ROADWAY	T	2	16,092.94	09/22/2011	12	94.00
NW15THTER (NW 15TH TER)	0010	01/01/1997	AC	ROADWAY	S	2	19,076.05	09/22/2011	14	100.00
NW17THST (NW 17TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	28,781.00	09/22/2011	14	92.00

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW17THST (NW 17TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	19,726.74	01/01/1997	0	100.00
NW17THST (NW 17TH ST)	0030	01/01/1995	AC	ROADWAY	S	2	44,154.39	09/22/2011	16	83.00
NW17THST (NW 17TH ST)	0040	01/01/1995	AC	ROADWAY	S	2	23,475.97	09/24/2011	16	90.00
NW18THTER (NW 18TH TER)	0010	01/01/1997	AC	ROADWAY	T	2	41,619.05	09/22/2011	14	91.00
NW18THTER (NW 18TH TER)	0020	01/01/1997	AC	ROADWAY	T	2	14,976.23	09/22/2011	14	90.00
NW19THST (NW 19TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	10,962.38	09/22/2011	14	98.00
NW19THST (NW 19TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	59,895.64	09/22/2011	14	95.00
NW20THST (NW 20TH ST)	0010	01/01/1997	AC	ROADWAY	T	2	28,721.11	09/22/2011	14	70.00
NW21STST (NW 21ST ST)	0010	01/01/1997	AC	ROADWAY	S	2	16,211.93	09/22/2011	14	87.00
NW21STST (NW 21ST ST)	0020	01/01/1997	AC	ROADWAY	S	2	11,758.13	09/22/2011	14	84.00
NW21STST (NW 21ST ST)	0030	01/01/1997	AC	ROADWAY	S	2	20,351.25	09/24/2011	14	60.00
NW21STST (NW 21ST ST)	0040	01/01/1997	AC	ROADWAY	S	2	17,926.13	09/22/2011	14	95.00
NW21STST (NW 21ST ST)	0050	01/01/1997	AC	ROADWAY	S	2	26,289.26	09/24/2011	14	51.00
NW21STST (NW 21ST ST)	0060	01/01/1997	AC	ROADWAY	S	2	13,091.52	09/22/2011	14	100.00
NW21STST (NW 21ST ST)	0070	01/01/1997	AC	ROADWAY	S	2	5,771.54	09/22/2011	14	86.00
NW21STST (NW 21ST ST)	0080	01/01/1997	AC	ROADWAY	S	2	13,693.59	09/22/2011	14	82.00
NW21STST (NW 21ST ST)	0090	01/01/1997	AC	ROADWAY	S	2	25,147.78	09/22/2011	14	82.00
NW21STST (NW 21ST ST)	0100	01/01/1997	AC	ROADWAY	T	2	5,221.88	09/22/2011	14	86.00
NW21STST (NW 21ST ST)	0110	01/01/1997	AC	ROADWAY	S	2	23,073.90	09/24/2011	14	95.00
NW21STST (NW 21ST ST)	0120	01/01/1997	AC	ROADWAY	T	2	9,057.52	09/22/2011	14	86.00
NW21STTER (NW 21ST TER)	0010	01/01/1997	AC	ROADWAY	S	2	9,380.72	09/22/2011	14	80.00
NW21STTER (NW 21ST TER)	0020	01/01/1995	AC	ROADWAY	T	2	20,025.88	09/22/2011	16	78.00
NW23RDST (NW 23RD ST)	0010	01/01/1997	AC	ROADWAY	T	2	28,621.72	09/22/2011	14	76.00
NW23RDST (NW 23RD ST)	0020	01/01/1997	AC	ROADWAY	S	2	22,708.69	09/22/2011	14	86.00
NW24THTER (NW 24TH TER)	0010	01/01/1997	AC	ROADWAY	T	2	21,529.50	09/22/2011	14	83.00
NW25THTER (NW 25TH TER)	0010	01/01/2002	AC	ROADWAY	S	2	6,612.50	09/22/2011	9	68.00

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW25THTER (NW 25TH TER)	0020	01/01/2002	AC	ROADWAY	S	2	11,839.83	09/22/2011	9	96.00
NW26THST (NW 26TH ST)	0010	01/01/2002	AC	ROADWAY	T	2	13,716.41	09/22/2011	9	91.00
NW26THST (NW 26TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	30,935.18	09/22/2011	9	86.00
NW26THST (NW 26TH ST)	0030	01/01/2002	AC	ROADWAY	S	2	12,944.09	09/22/2011	9	84.00
NW26THST (NW 26TH ST)	0040	01/01/2002	AC	ROADWAY	S	2	13,036.25	09/22/2011	9	85.00
NW26THST (NW 26TH ST)	0050	01/01/2002	AC	ROADWAY	S	2	13,311.25	09/22/2011	9	90.00
NW26THST (NW 26TH ST)	0060	01/01/2002	AC	ROADWAY	S	2	12,994.43	09/22/2011	9	87.00
NW27THST (NW 27TH ST)	0010	01/01/1997	AC	ROADWAY	T	2	34,137.34	09/22/2011	14	97.00
NW27THST (NW 27TH ST)	0020	01/01/2010	AC	ROADWAY	S	2	12,721.65	09/22/2011	1	100.00
NW27THST (NW 27TH ST)	0030	01/01/2002	AC	ROADWAY	S	2	50,440.44	09/22/2011	9	78.00
NW27THST (NW 27TH ST)	0040	01/01/2002	AC	ROADWAY	S	2	31,924.06	09/22/2011	9	81.00
NW27THST (NW 27TH ST)	0050	01/01/2002	AC	ROADWAY	S	2	13,027.38	09/22/2011	9	91.00
NW27THST (NW 27TH ST)	0060	01/01/2002	AC	ROADWAY	S	2	33,710.40	09/22/2011	9	78.00
NW27THST (NW 27TH ST)	0070	01/01/2002	AC	ROADWAY	S	2	11,552.53	09/22/2011	9	84.00
NW27THST (NW 27TH ST)	0080	01/01/2010	AC	ROADWAY	S	2	13,474.24	09/22/2011	1	86.00
NW27THST (NW 27TH ST)	0090	01/01/2010	AC	ROADWAY	T	2	29,244.37	09/22/2011	1	100.00
NW27THST (NW 27TH ST)	0100	01/01/1997	AC	ROADWAY	T	2	15,891.01	09/22/2011	14	100.00
NW27THST (NW 27TH ST)	0110	01/01/1997	AC	ROADWAY	S	4	28,511.76	09/22/2011	14	100.00
NW27THST (NW 27TH ST)	0120	01/01/2002	AC	ROADWAY	S	2	12,987.51	09/22/2011	9	83.00
NW27THTER (NW 27TH TER)	0010	01/01/2010	AC	ROADWAY	S	2	15,532.91	09/22/2011	1	100.00
NW27THTER (NW 27TH TER)	0020	01/01/2010	AC	ROADWAY	S	2	13,225.08	09/22/2011	1	100.00
NW27THTER (NW 27TH TER)	0030	01/01/2010	AC	ROADWAY	S	2	13,175.14	09/22/2011	1	100.00
NW27THTER (NW 27TH TER)	0040	01/01/2010	AC	ROADWAY	S	2	12,865.15	09/22/2011	1	100.00
NW28THST (NW 28TH ST)	0010	01/01/2002	AC	ROADWAY	T	2	13,548.10	09/22/2011	9	97.00
NW28THTER (NW 28TH TER)	0010	01/01/2010	AC	ROADWAY	S	2	12,908.97	09/22/2011	1	100.00
NW28THTER (NW 28TH TER)	0020	01/01/2010	AC	ROADWAY	S	2	13,225.08	09/22/2011	1	100.00
NW28THTER (NW 28TH TER)	0030	01/01/2010	AC	ROADWAY	S	2	13,131.58	09/22/2011	1	80.00

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NW28THTER (NW 28TH TER)	0040	01/01/2010	AC	ROADWAY	S	2	13,406.25	09/22/2011	1	100.00
NW29THST (NW 29TH ST)	0010	01/01/1999	AC	ROADWAY	S	2	24,703.64	09/23/2011	12	100.00
NW29THST (NW 29TH ST)	0020	01/01/1999	AC	ROADWAY	S	2	39,409.73	09/22/2011	12	77.00
NW29THST (NW 29TH ST)	0030	01/01/1999	AC	ROADWAY	S	2	17,266.89	09/22/2011	12	86.00
NW29THST (NW 29TH ST)	0040	01/01/1999	AC	ROADWAY	T	2	14,314.15	09/22/2011	12	95.00
NW29THST (NW 29TH ST)	0050	01/01/1999	AC	ROADWAY	T	2	28,032.11	09/22/2011	12	91.00
NW29THST (NW 29TH ST)	0060	01/01/1999	AC	ROADWAY	T	2	21,023.34	09/22/2011	12	87.00
NW29THST (NW 29TH ST)	0080	01/01/1999	AC	ROADWAY	S	2	39,116.57	09/24/2011	12	92.00
NW29THST (NW 29TH ST)	0090	01/01/1999	AC	ROADWAY	S	2	18,984.27	09/22/2011	12	80.00
NW29THTER (NW 29TH TER)	0010	01/01/1997	AC	ROADWAY	S	2	36,035.63	09/24/2011	14	82.00
NW30THST (NW 30TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	26,602.59	09/22/2011	14	99.00
NW30THST (NW 30TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	36,576.41	09/22/2011	14	99.00
NW30THTER (NW 30TH TER)	0010	01/01/1997	AC	ROADWAY	S	2	40,305.14	09/24/2011	14	71.00
NW30THTER (NW 30TH TER)	0020	01/01/1997	AC	ROADWAY	S	2	12,088.07	09/24/2011	14	84.00
NW30THTER (NW 30TH TER)	0030	01/01/1997	AC	ROADWAY	S	2	17,664.09	09/24/2011	14	78.00
NW31STST (NW 31ST ST)	0010	01/01/1995	AC	ROADWAY	S	2	29,029.38	09/22/2011	16	91.00
NW31STST (NW 31ST ST)	0020	01/01/1999	AC	ROADWAY	S	2	9,796.65	09/23/2011	12	95.00
NW31STTER (NW 31ST TER)	0010	01/01/1999	AC	ROADWAY	S	2	20,529.18	09/23/2011	12	93.00
NW31STTER (NW 31ST TER)	0020	01/01/1999	AC	ROADWAY	S	2	4,345.35	01/01/1999	0	100.00
NW33RDST (NW 33RD ST)	0010	01/01/2002	AC	ROADWAY	P	2	21,187.38	09/22/2011	9	81.00
NW33RDST (NW 33RD ST)	0020	01/01/2001	AC	ROADWAY	S	2	18,675.68	09/24/2011	10	75.00
NW33RDST (NW 33RD ST)	0030	01/01/2010	AC	ROADWAY	S	2	19,183.98	09/24/2011	1	91.00
NW33RDST (NW 33RD ST)	0040	01/01/2010	AC	ROADWAY	S	2	8,863.26	09/24/2011	1	93.00
NW33RDST (NW 33RD ST)	0050	01/01/2010	AC	ROADWAY	S	2	31,117.05	09/24/2011	1	99.00
NW33RDST (NW 33RD ST)	0060	01/01/2001	AC	ROADWAY	S	2	13,917.31	09/24/2011	10	83.00
NW33RDST (NW 33RD ST)	0070	01/01/2001	AC	ROADWAY	S	2	14,418.41	09/24/2011	10	66.00

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NW33RDST (NW 33RD ST)	0080	01/01/2002	AC	ROADWAY	P	2	28,464.59	09/22/2011	9	98.00
NW33RDST (NW 33RD ST)	0090	01/01/2001	AC	ROADWAY	S	2	12,641.25	09/24/2011	10	67.00
NW33RDST (NW 33RD ST)	0100	01/01/2010	AC	ROADWAY	P	2	19,923.91	09/22/2011	1	100.00
NW33RDST (NW 33RD ST)	0110	01/01/2001	AC	ROADWAY	S	2	40,025.75	09/24/2011	10	73.00
NW33RDST (NW 33RD ST)	0120	01/01/2002	AC	ROADWAY	S	2	18,058.83	09/22/2011	9	92.00
NW33RDST (NW 33RD ST)	0130	01/01/2002	AC	ROADWAY	S	2	37,046.25	09/22/2011	9	93.00
NW33RDST (NW 33RD ST)	0140	01/01/2002	AC	ROADWAY	S	2	26,265.00	09/22/2011	9	95.00
NW33RDST (NW 33RD ST)	0150	01/01/2002	AC	ROADWAY	P	2	10,119.38	09/22/2011	9	87.00
NW33RDST (NW 33RD ST)	0160	01/01/2002	AC	ROADWAY	P	2	19,757.57	09/22/2011	9	93.00
NW33RDST (NW 33RD ST)	0170	01/01/2002	AC	ROADWAY	P	2	18,217.62	09/22/2011	9	86.00
NW33RDST (NW 33RD ST)	0180	01/01/2002	AC	ROADWAY	P	2	39,796.88	09/22/2011	9	84.00
NW34THST (NW 34TH ST)	0010	01/01/2002	AC	ROADWAY	S	2	15,321.00	09/22/2011	9	93.00
NW34THST (NW 34TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	23,883.00	09/22/2011	9	100.00
NW34THST (NW 34TH ST)	0030	01/01/2002	AC	ROADWAY	S	2	15,672.00	09/22/2011	9	100.00
NW34THST (NW 34TH ST)	0040	01/01/2002	AC	ROADWAY	S	2	21,045.00	09/22/2011	9	100.00
NW35THLN (NW 35TH LN)	0010	01/01/2002	AC	ROADWAY	S	2	41,860.27	09/24/2011	9	79.00
NW36THST (NW 36TH ST)	0010	01/01/2001	AC	ROADWAY	S	2	39,358.23	09/23/2011	10	100.00
NW36THTER (NW 36TH TER)	0010	01/01/2000	AC	ROADWAY	S	2	14,919.46	09/22/2011	11	90.00
NW37THST (NW 37TH ST)	0010	01/01/2002	AC	ROADWAY	S	2	13,206.46	09/24/2011	9	52.00
NW37THST (NW 37TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	5,596.75	09/24/2011	9	50.00
NW37THTER (NW 37TH TER)	0010	01/01/2000	AC	ROADWAY	S	2	35,952.86	09/23/2011	11	99.00
NW38THST (NW 38TH ST)	0010	01/01/2002	AC	ROADWAY	S	2	13,273.78	09/24/2011	9	36.00
NW38THST (NW 38TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	20,628.45	09/24/2011	9	66.00
NW40THST (NW 40TH STREET RD)	0010	01/01/2002	AC	ROADWAY	S	2	15,327.21	09/24/2011	9	64.00
NW40THST (NW 40TH STREET RD)	0020	01/01/2002	AC	ROADWAY	S	2	10,592.11	09/24/2011	9	73.00
NW41STST (NW 41ST ST)	0010	01/01/2002	AC	ROADWAY	S	2	7,748.17	09/23/2011	9	14.00

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NW41STST (NW 41ST ST)	0020	01/01/2002	AC	ROADWAY	S	2	21,851.93	09/23/2011	9	55.00
NW41STST (NW 41ST ST)	0040	01/01/2002	AC	ROADWAY	S	2	23,839.75	09/23/2011	9	63.00
NW41STST (NW 41ST ST)	0080	01/01/2002	AC	ROADWAY	S	2	33,950.31	09/23/2011	9	45.00
NW50THST (NW 50TH ST)	0010	01/01/2002	AC	ROADWAY	S	2	26,439.86	09/23/2011	9	81.00
NW50THST (NW 50TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	17,479.78	09/23/2011	9	100.00
NW50THST (NW 50TH ST)	0030	01/01/2002	AC	ROADWAY	S	2	26,987.80	09/23/2011	9	90.00
NW50THST (NW 50TH ST)	0040	01/01/2002	AC	ROADWAY	S	2	39,291.03	09/23/2011	9	100.00
NW50THST (NW 50TH ST)	0050	01/01/2002	AC	ROADWAY	S	2	15,801.28	09/23/2011	9	98.00
NW50THST (NW 50TH ST)	0060	01/01/1997	AC	ROADWAY	S	2	21,929.63	09/23/2011	14	64.00
NW50THST (NW 50TH ST)	0070	01/01/2002	AC	ROADWAY	S	2	5,775.70	09/23/2011	9	97.00
NW52NDST (NW 52ND ST)	0020	01/01/1999	AC	ROADWAY	P	2	21,744.87	09/23/2011	12	91.00
NW52NDST (NW 52ND ST)	0030	01/01/1999	AC	ROADWAY	S	2	2,254.50	09/23/2011	12	97.00
NW52NDST (NW 52ND ST)	0040	01/01/1999	AC	ROADWAY	S	2	4,188.78	09/23/2011	12	96.00
NW52NDST (NW 52ND ST)	0050	01/01/1999	AC	ROADWAY	S	2	2,740.20	09/23/2011	12	93.00
NW52NDST (NW 52ND ST)	0070	01/01/1995	AC	ROADWAY	S	2	10,753.00	09/23/2011	16	73.00
NW52NDST (NW 52ND ST)	0080	01/01/1999	AC	ROADWAY	P	2	10,052.14	09/23/2011	12	99.00
NW52NDST (NW 52ND ST)	0090	01/01/1999	AC	ROADWAY	P	2	22,078.97	09/23/2011	12	94.00
NW52NDST (NW 52ND ST)	0110	01/01/1999	AC	ROADWAY	P	2	15,899.31	09/23/2011	12	99.00
NW52NDST (NW 52ND ST)	0120	01/01/1999	AC	ROADWAY	S	2	6,444.50	09/23/2011	12	80.00
NW52NDST (NW 52ND ST)	0130	01/01/1999	AC	ROADWAY	S	2	2,335.41	01/01/1999	0	100.00
NW52NDST (NW 52ND ST)	0140	01/01/1999	AC	ROADWAY	P	2	12,036.99	09/23/2011	12	99.00
NW52NDST (NW 52ND ST)	0150	01/01/1999	AC	ROADWAY	S	2	13,430.51	09/23/2011	12	73.00
NW52NDST (NW 52ND ST)	0160	01/01/1999	AC	ROADWAY	P	2	6,883.01	09/23/2011	12	89.00
NW52NDST (NW 52ND ST)	0170	01/01/1999	AC	ROADWAY	P	2	6,944.06	09/23/2011	12	74.00
NW52NDST (NW 52ND ST)	0200	01/01/1999	AC	ROADWAY	P	2	11,106.22	09/23/2011	12	93.00
NW52NDST (NW 52ND ST)	0210	01/01/1999	AC	ROADWAY	P	2	24,121.87	09/23/2011	12	83.00
NW52NDST (NW 52ND ST)	0220	01/01/1999	AC	ROADWAY	P	2	18,636.03	09/23/2011	12	98.00

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NW52NDST (NW 52ND ST)	0230	01/01/1999	AC	ROADWAY	S	2	6,039.00	09/23/2011	12	74.00
NW52NDTER (NW 52ND TER)	0010	01/01/1999	AC	ROADWAY	S	2	3,247.75	01/01/1999	0	100.00
NW52NDTER (NW 52ND TER)	0020	01/01/1999	AC	ROADWAY	S	2	26,967.65	09/23/2011	12	81.00
NW53RDST (NW 53RD ST)	0020	01/01/1997	AC	ROADWAY	S	2	13,540.46	09/23/2011	14	73.00
NW53RDST (NW 53RD ST)	0080	01/01/1997	AC	ROADWAY	S	2	5,539.17	09/23/2011	14	93.00
NW53RDST (NW 53RD ST)	0090	01/01/1997	AC	ROADWAY	S	2	19,322.43	09/23/2011	14	88.00
NW53RDST (NW 53RD ST)	0100	01/01/1997	AC	ROADWAY	S	2	11,400.63	09/23/2011	14	77.00
NW53RDST (NW 53RD ST)	0110	01/01/1997	AC	ROADWAY	S	2	32,164.10	09/23/2011	14	92.00
NW53RDST (NW 53RD ST)	0120	01/01/1997	AC	ROADWAY	S	2	4,295.00	09/23/2011	14	82.00
NW53RDST (NW 53RD ST)	0130	01/01/1997	AC	ROADWAY	S	2	4,966.80	09/23/2011	14	86.00
NW53RDST (NW 53RD ST)	0140	01/01/1997	AC	ROADWAY	S	2	8,355.00	09/23/2011	14	66.00
NW53RDST (NW 53RD ST)	0160	01/01/1999	AC	ROADWAY	S	2	10,725.18	09/23/2011	12	100.00
NW53RDTER (NW 53RD TER)	0010	01/01/1997	AC	ROADWAY	S	2	24,808.61	09/23/2011	14	83.00
NW54THST (NW 54TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	26,494.86	09/23/2011	14	58.00
NW54THST (NW 54TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	13,355.39	09/23/2011	14	90.00
NW54THST (NW 54TH ST)	0030	01/01/1997	AC	ROADWAY	S	2	10,883.96	09/23/2011	14	88.00
NW54THST (NW 54TH ST)	0040	01/01/1997	AC	ROADWAY	S	2	26,372.57	09/23/2011	14	67.00
NW54THST (NW 54TH ST)	0050	01/01/1997	AC	ROADWAY	S	2	26,276.29	09/23/2011	14	72.00
NW55THST (NW 55TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	10,853.56	09/23/2011	14	90.00
NW55THST (NW 55TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	13,243.94	09/23/2011	14	89.00
NW56THST (NW 56TH ST)	0010	01/01/1997	AC	ROADWAY	S	2	29,211.72	09/23/2011	14	84.00
NW56THST (NW 56TH ST)	0020	01/01/1997	AC	ROADWAY	S	2	28,912.13	09/23/2011	14	74.00
NW56THST (NW 56TH ST)	0030	01/01/1997	AC	ROADWAY	S	2	29,143.12	09/23/2011	14	64.00
NW56THST (NW 56TH ST)	0040	01/01/1997	AC	ROADWAY	S	2	14,634.12	09/23/2011	14	85.00
NW56THST (NW 56TH ST)	0050	01/01/1997	AC	ROADWAY	S	2	11,017.06	09/23/2011	14	95.00
NW57THST (NW 57TH ST)	0010	01/01/2006	AC	ROADWAY	S	2	5,157.63	09/23/2011	5	100.00

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NW57THST (NW 57TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	14,803.35	09/23/2011	9	100.00
NW66THST (NW 66TH ST)	0010	01/01/2002	AC	ROADWAY	S	2	31,156.92	01/01/2002	0	100.00
NW66THST (NW 66TH ST)	0020	01/01/2002	AC	ROADWAY	S	2	51,835.55	09/22/2011	9	93.00
NW77THCT (NW 77TH CT)	0010	01/01/1995	AC	ROADWAY	S	2	6,416.44	09/23/2011	16	62.00
NW77THCT (NW 77TH CT)	0020	01/01/1995	AC	ROADWAY	S	2	6,623.61	09/23/2011	16	64.00
NW77THCT (NW 77TH CT)	0030	01/01/2001	AC	ROADWAY	S	2	6,911.43	09/23/2011	10	66.00
NW77THCT (NW 77TH CT)	0040	01/01/2001	AC	ROADWAY	S	2	6,953.17	09/23/2011	10	75.00
NW77THCT (NW 77TH CT)	0050	01/01/2003	AC	ROADWAY	S	2	19,197.49	09/22/2011	8	71.00
NW78THAVE (NW 78TH AVE)	0010	01/01/2001	AC	ROADWAY	S	2	7,198.84	09/23/2011	10	84.00
NW78THAVE (NW 78TH AVE)	0020	01/01/2001	AC	ROADWAY	S	2	7,402.15	09/23/2011	10	100.00
NW78THAVE (NW 78TH AVE)	0030	01/01/2003	AC	ROADWAY	T	2	59,594.62	09/24/2011	8	68.00
NW78THAVE (NW 78TH AVE)	0040	01/01/2003	AC	ROADWAY	S	2	6,641.91	09/23/2011	8	98.00
NW78THAVE (NW 78TH AVE)	0070	01/01/2001	AC	ROADWAY	S	2	7,276.50	09/23/2011	10	89.00
NW78THAVE (NW 78TH AVE)	0080	01/01/2003	AC	ROADWAY	S	2	5,842.13	09/24/2011	8	51.00
NW79THAVE (NW 79TH AVE)	0010	01/01/2010	AC	ROADWAY	P	4	33,928.63	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0020	01/01/2010	AC	ROADWAY	P	4	16,746.51	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0030	01/01/2010	AC	ROADWAY	P	4	69,242.88	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0040	01/01/2010	AC	ROADWAY	P	4	13,084.34	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0050	01/01/2010	AC	ROADWAY	P	4	13,653.79	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0060	01/01/2010	AC	ROADWAY	P	4	2,952.07	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0070	01/01/2010	AC	ROADWAY	P	4	2,965.63	01/01/2010	0	100.00
NW79THAVE (NW 79TH AVE)	0080	01/01/2010	AC	ROADWAY	P	4	16,957.65	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0090	01/01/2010	AC	ROADWAY	P	4	16,188.87	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0100	01/01/2010	AC	ROADWAY	P	4	19,695.94	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0110	01/01/2010	AC	ROADWAY	P	4	16,825.15	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0120	01/01/2010	AC	ROADWAY	P	4	15,345.64	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0130	01/01/2010	AC	ROADWAY	P	4	25,008.39	09/21/2011	1	100.00

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NW79THAVE (NW 79TH AVE)	0140	01/01/2010	AC	ROADWAY	P	4	16,711.30	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0150	01/01/2010	AC	ROADWAY	P	4	16,375.28	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0160	01/01/1997	AC	ROADWAY	S	2	19,832.69	09/24/2011	14	62.00
NW79THAVE (NW 79TH AVE)	0170	01/01/1997	AC	ROADWAY	S	2	19,519.77	09/24/2011	14	65.00
NW79THAVE (NW 79TH AVE)	0180	01/01/2010	AC	ROADWAY	P	4	16,273.07	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0190	01/01/2010	AC	ROADWAY	P	4	11,107.43	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0200	01/01/2010	AC	ROADWAY	P	4	2,330.36	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0210	01/01/2010	AC	ROADWAY	P	4	18,611.57	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0220	01/01/2010	AC	ROADWAY	P	4	15,466.48	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0230	01/01/2010	AC	ROADWAY	P	4	31,833.56	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0240	01/01/2010	AC	ROADWAY	P	4	8,679.55	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0250	01/01/2010	AC	ROADWAY	P	4	8,926.97	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0260	01/01/2010	AC	ROADWAY	P	4	18,676.25	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0270	01/01/2010	AC	ROADWAY	P	4	22,581.10	09/21/2011	1	96.00
NW79THAVE (NW 79TH AVE)	0280	01/01/2010	AC	ROADWAY	P	4	7,959.69	09/21/2011	1	94.00
NW79THAVE (NW 79TH AVE)	0290	01/01/2010	AC	ROADWAY	P	4	21,913.72	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0300	01/01/2010	AC	ROADWAY	P	4	17,097.22	09/21/2011	1	99.00
NW79THAVE (NW 79TH AVE)	0310	01/01/2010	AC	ROADWAY	P	4	8,469.08	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0320	01/01/2010	AC	ROADWAY	P	4	7,938.86	01/01/2010	0	100.00
NW79THAVE (NW 79TH AVE)	0330	01/01/2010	AC	ROADWAY	P	4	19,277.67	09/21/2011	1	100.00
NW79THAVE (NW 79TH AVE)	0340	01/01/1997	AC	ROADWAY	S	2	22,394.54	09/24/2011	14	71.00
NW79THAVE (NW 79TH AVE)	0350	01/01/1997	AC	ROADWAY	S	2	16,328.75	09/24/2011	14	79.00
NW82NDAVE (NW 82ND AVE)	0010	01/01/2001	AC	ROADWAY	S	2	15,938.98	09/23/2011	10	77.00
NW82NDAVE (NW 82ND AVE)	0020	01/01/2009	AC	ROADWAY	P	2	16,236.08	09/24/2011	2	85.00
NW82NDAVE (NW 82ND AVE)	0030	01/01/2001	AC	ROADWAY	S	2	15,832.37	09/23/2011	10	90.00
NW82NDAVE (NW 82ND AVE)	0040	01/01/1995	AC	ROADWAY	P	2	23,366.31	09/24/2011	16	55.00
NW82NDAVE (NW 82ND AVE)	0050	01/01/1995	AC	ROADWAY	P	2	24,585.54	09/24/2011	16	76.00

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NW82NDAVE (NW 82ND AVE)	0060	01/01/1995	AC	ROADWAY	P	2	10,565.59	09/24/2011	16	92.00
NW82NDAVE (NW 82ND AVE)	0070	01/01/2009	AC	ROADWAY	P	2	13,699.42	09/24/2011	2	99.00
NW82NDAVE (NW 82ND AVE)	0080	01/01/1995	AC	ROADWAY	P	2	16,777.11	09/24/2011	16	82.00
NW82NDAVE (NW 82ND AVE)	0090	01/01/2009	AC	ROADWAY	P	2	34,472.71	09/24/2011	2	80.00
NW82NDAVE (NW 82ND AVE)	0100	01/01/2009	AC	ROADWAY	P	2	20,885.43	09/24/2011	2	88.00
NW82NDAVE (NW 82ND AVE)	0110	01/01/1995	AC	ROADWAY	P	2	19,666.84	09/24/2011	16	93.00
NW82NDAVE (NW 82ND AVE)	0120	01/01/1995	AC	ROADWAY	T	2	21,627.80	09/24/2011	16	70.00
NW82NDAVE (NW 82ND AVE)	0130	01/01/2009	AC	ROADWAY	P	2	7,317.84	09/24/2011	2	100.00
NW82NDAVE (NW 82ND AVE)	0140	01/01/1995	AC	ROADWAY	P	2	16,423.68	09/24/2011	16	63.00
NW82NDAVE (NW 82ND AVE)	0150	01/01/1995	AC	ROADWAY	P	2	10,159.60	09/24/2011	16	75.00
NW82NDAVE (NW 82ND AVE)	0160	01/01/1995	AC	ROADWAY	P	2	5,805.88	09/24/2011	16	89.00
NW82NDAVE (NW 82ND AVE)	0170	01/01/1995	AC	ROADWAY	P	2	9,345.44	09/24/2011	16	99.00
NW82NDAVE (NW 82ND AVE)	0180	01/01/2009	AC	ROADWAY	P	2	22,529.09	09/24/2011	2	81.00
NW82NDAVE (NW 82ND AVE)	0190	01/01/1995	AC	ROADWAY	P	2	19,520.79	09/24/2011	16	91.00
NW82NDAVE (NW 82ND AVE)	0200	01/01/1995	AC	ROADWAY	P	2	4,377.25	09/24/2011	16	100.00
NW82NDST (NW 82ND ST)	0010	01/01/1997	AC	ROADWAY	T	4	48,604.76	09/23/2011	14	93.00
NW82NDST (NW 82ND ST)	0020	01/01/1997	AC	ROADWAY	T	2	42,487.11	09/23/2011	14	94.00
NW82NDST (NW 82ND ST)	0030	01/01/1997	AC	ROADWAY	T	2	74,357.33	09/23/2011	14	98.00
NW84THAVE (NW 84TH AVE)	0010	01/01/1997	AC	ROADWAY	S	2	18,117.39	09/24/2011	14	68.00
NW84THAVE (NW 84TH AVE)	0020	01/01/1997	AC	ROADWAY	S	2	32,681.27	09/24/2011	14	65.00
NW84THAVE (NW 84TH AVE)	0030	01/01/2010	AC	ROADWAY	S	2	16,495.06	09/22/2011	1	93.00
NW84THAVE (NW 84TH AVE)	0040	01/01/1995	AC	ROADWAY	S	2	39,571.24	09/22/2011	16	88.00
NW84THAVE (NW 84TH AVE)	0050	01/01/1999	AC	ROADWAY	S	2	4,449.25	09/23/2011	12	87.00
NW84THAVE (NW 84TH AVE)	0060	01/01/2003	AC	ROADWAY	S	2	15,941.16	09/23/2011	8	100.00
NW84THAVE (NW 84TH AVE)	0070	01/01/1995	AC	ROADWAY	S	2	32,771.56	09/22/2011	16	86.00
NW84THAVE (NW 84TH AVE)	0080	01/01/1999	AC	ROADWAY	S	2	8,045.13	09/23/2011	12	88.00
NW84THAVE (NW 84TH AVE)	0090	01/01/1995	AC	ROADWAY	S	2	22,450.06	09/22/2011	16	87.00

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NW84THAVE (NW 84TH AVE)	0100	01/01/1999	AC	ROADWAY	S	2	2,879.25	01/01/1999	0	100.00
NW84THAVE (NW 84TH AVE)	0110	01/01/2003	AC	ROADWAY	S	2	15,729.64	09/23/2011	8	97.00
NW84THAVE (NW 84TH AVE)	0120	01/01/1995	AC	ROADWAY	S	2	5,586.24	09/22/2011	16	100.00
NW84THAVE (NW 84TH AVE)	0130	01/01/1995	AC	ROADWAY	S	2	12,725.95	09/22/2011	16	92.00
NW84THAVE (NW 84TH AVE)	0140	01/01/1997	AC	ROADWAY	S	2	10,471.99	09/24/2011	14	68.00
NW84THAVE (NW 84TH AVE)	0150	01/01/1997	AC	ROADWAY	S	2	14,817.80	09/24/2011	14	72.00
NW84THAVE (NW 84TH AVE)	0160	01/01/1995	AC	ROADWAY	S	2	24,224.83	09/22/2011	16	91.00
NW84THAVE (NW 84TH AVE)	0170	01/01/1999	AC	ROADWAY	S	2	13,371.24	09/23/2011	12	65.00
NW86THAVE (NW 86TH AVE)	0010	01/01/2003	AC	ROADWAY	S	2	12,162.04	09/22/2011	8	86.00
NW86THST (NW 86TH ST)	0010	01/01/2005	AC	ROADWAY	S	2	8,102.42	09/23/2011	6	80.00
NW86THST (NW 86TH ST)	0020	01/01/2005	AC	ROADWAY	S	2	4,366.59	09/23/2011	6	100.00
NW86THST (NW 86TH ST)	0030	01/01/2005	AC	ROADWAY	S	2	17,894.53	09/23/2011	6	92.00
NW86THST (NW 86TH ST)	0040	01/01/2005	AC	ROADWAY	S	2	4,516.57	09/23/2011	6	98.00
NW86THST (NW 86TH ST)	0050	01/01/2005	AC	ROADWAY	S	2	8,196.16	09/23/2011	6	83.00
NW86THST (NW 86TH ST)	0060	01/01/2005	AC	ROADWAY	S	2	8,748.78	09/23/2011	6	100.00
NW86THST (NW 86TH ST)	0070	01/01/2005	AC	ROADWAY	S	2	13,016.73	09/23/2011	6	100.00
NW87THCT (NW 87TH CT)	0010	01/01/2003	AC	ROADWAY	S	2	17,076.01	09/22/2011	8	86.00
NW88THAVE (NW 88TH AVE)	0010	01/01/1997	AC	ROADWAY	T	2	5,787.43	09/22/2011	14	93.00
NW88THAVE (NW 88TH AVE)	0020	01/01/1995	AC	ROADWAY	T	2	18,985.32	09/22/2011	16	82.00
NW88THCT (NW 88TH CT)	0010	01/01/1997	AC	ROADWAY	T	2	20,285.14	09/22/2011	14	71.00
NW88THCT (NW 88TH CT)	0020	01/01/1997	AC	ROADWAY	T	2	14,459.89	09/22/2011	14	85.00
NW88THCT (NW 88TH CT)	0030	01/01/1997	AC	ROADWAY	T	2	13,466.11	09/22/2011	14	63.00
NW89THCT (NW 89TH CT)	0010	01/01/1992	AC	ROADWAY	T	2	6,548.62	09/22/2011	19	65.00
NW89THCT (NW 89TH CT)	0020	01/01/1999	AC	ROADWAY	S	2	20,993.05	09/24/2011	12	84.00
NW89THCT (NW 89TH CT)	0030	01/01/1999	AC	ROADWAY	S	2	16,601.86	09/22/2011	12	82.00
NW89THCT (NW 89TH CT)	0040	01/01/1999	AC	ROADWAY	S	2	13,943.54	09/22/2011	12	94.00

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW89THCT (NW 89TH CT)	0050	01/01/1992	AC	ROADWAY	T	2	18,989.64	09/22/2011	19	89.00
NW89THCT (NW 89TH CT)	0060	01/01/1992	AC	ROADWAY	T	2	15,375.17	09/22/2011	19	87.00
NW89THPL (NW 89TH PL)	0010	01/01/1999	AC	ROADWAY	T	2	21,482.88	09/22/2011	12	94.00
NW89THPL (NW 89TH PL)	0020	01/01/1999	AC	ROADWAY	T	2	18,244.31	09/22/2011	12	92.00
NW89THPL (NW 89TH PL)	0030	01/01/1999	AC	ROADWAY	T	2	27,924.55	09/22/2011	12	92.00
NW89THPL (NW 89TH PL)	0040	01/01/1999	AC	ROADWAY	T	2	2,042.83	09/22/2011	12	73.00
NW91STAVE (NW 91ST AVE)	0010	01/01/1999	AC	ROADWAY	S	2	17,736.60	09/24/2011	12	87.00
NW92NDAVE (NW 92ND AVE)	0010	01/01/2000	AC	ROADWAY	S	2	18,868.12	09/23/2011	11	70.00
NW92NDAVE (NW 92ND AVE)	0030	01/01/2000	AC	ROADWAY	S	2	12,764.40	09/22/2011	11	87.00
NW92NDAVE (NW 92ND AVE)	0040	01/01/2000	AC	ROADWAY	S	2	16,652.44	09/22/2011	11	90.00
NW92NDAVE (NW 92ND AVE)	0050	01/01/2000	AC	ROADWAY	S	2	12,709.36	09/22/2011	11	86.00
NW93RDCT (NW 93RD CT)	0010	01/01/2010	AC	ROADWAY	T	2	8,395.74	09/22/2011	1	100.00
NW93RDCT (NW 93RD CT)	0020	01/01/2010	AC	ROADWAY	T	2	5,915.62	09/22/2011	1	100.00
NW98THAVE (NW 98TH AVE)	0020	01/01/2010	AC	ROADWAY	S	2	6,089.36	09/22/2011	1	100.00
NW98THAVE (NW 98TH AVE)	0030	01/01/2010	AC	ROADWAY	S	2	5,945.75	09/22/2011	1	100.00
NW98THAVE (NW 98TH AVE)	0040	01/01/2010	AC	ROADWAY	S	2	5,257.77	09/22/2011	1	100.00
NW98THAVE (NW 98TH AVE)	0050	01/01/2010	AC	ROADWAY	S	2	5,791.98	09/22/2011	1	100.00
NW98THCT (NW 98TH CT)	0010	01/01/1999	AC	ROADWAY	S	2	36,238.53	09/22/2011	12	93.00
NW98THCT (NW 98TH CT)	0020	01/01/1999	AC	ROADWAY	S	2	36,546.12	09/22/2011	12	100.00
NW99THAVE (NW 99TH AVE)	0010	01/01/2010	AC	ROADWAY	S	2	5,296.69	09/22/2011	1	100.00
NW99THAVE (NW 99TH AVE)	0020	01/01/2000	AC	ROADWAY	S	2	19,967.75	09/24/2011	11	62.00
NW99THAVE (NW 99TH AVE)	0030	01/01/2010	AC	ROADWAY	S	2	5,322.60	09/22/2011	1	100.00
NW99THAVE (NW 99TH AVE)	0040	01/01/2010	AC	ROADWAY	S	2	2,278.80	09/22/2011	1	100.00
NW99THAVE (NW 99TH AVE)	0050	01/01/1999	AC	ROADWAY	T	2	37,183.57	09/22/2011	12	68.00
NW99THAVE (NW 99TH AVE)	0060	01/01/2010	AC	ROADWAY	S	2	4,818.34	09/22/2011	1	100.00
NW99THAVE (NW 99TH AVE)	0070	01/01/2010	AC	ROADWAY	S	2	5,343.84	09/22/2011	1	100.00

Date: 1 /6/2012

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Pavement Database: DORAL_CLEAN_COPY NetworkID: CITYROAD

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Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	Lanes	True Area (SqFt)	Last Inspection Date	Age At Inspection	PCI
NW99THAVE (NW 99TH AVE)	0080	01/01/2010	AC	ROADWAY	S	2	2,703.84	09/22/2011	1	100.00
NW99THAVE (NW 99TH AVE)	0090	01/01/2000	AC	ROADWAY	S	2	8,934.79	09/24/2011	11	88.00

Age Category	Average Age At Inspection	Total Area (SqFt)	Number of Sections	Arithmetic Average PCI	PCI Standard Deviation	Weighted Average PCI
0-02	0.95	1,293,818.43	87	98.11	5.01	97.77
03-05	5.00	5,157.63	1	100.00	0.00	100.00
06-10	8.49	2,587,564.48	162	85.30	14.56	85.93
11-15	12.97	2,986,957.83	146	84.25	11.98	84.85
16-20	16.40	532,547.89	30	81.87	12.32	82.39
All	9.04	7,406,046.26	426	87.35	13.29	87.32

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Branch ID	Section ID	From	To	PCI_2007	PCI_2011
NW100THAVE	10	NW 25TH TER	NW 26TH ST	87	93
NW100THAVE	20	NW 27TH ST	NW 27TH TER	70.4	68
NW100THAVE	30	NW 27TH TER	NW 28TH TER	93	70
NW100THAVE	40	NW 28TH TER	Dead End	93	100
NW100THAVE	50	NW 14TH ST	NW 15TH TER	75.2	91
NW100THAVE	60	NW 26TH ST	NW 27TH ST	88.8	79
NW102NDAVE	70	NW 41ST ST	NW 43RD TER	69.7	71
NW102NDAVE	90	NW 15TH TER	NW 19TH ST	72.1	95
NW102NDAVE	100	NW 58TH ST	Dead End	46.2	97
NW102NDAVE	110	NW 27TH TER	NW 28TH TER	93	68
NW102NDAVE	120	NW 21ST ST	NW 25TH ST	38.8	100
NW102NDAVE	130	NW 27TH ST	NW 27TH TER	41.9	57
NW102NDAVE	140	NW 25TH ST	117 ft N of NW 25TH ST	92.9	73
NW102NDAVE	150	258 ft S of NW 26TH ST	NW 26TH ST	92.9	73
NW102NDAVE	160	NW 26TH ST	NW 27TH ST	90.6	74
NW102NDAVE	170	NW 48TH STREET CIR	NW 49TH LN	74	97
NW102NDAVE	180	NW 19TH ST	NW 21ST ST	88.8	99
NW102NDAVE	190	NW 56TH TER	NW 58TH ST	93	78
NW102NDAVE	200	NW 46TH ST	NW 102ND AVE	74	85
NW102NDAVE	210	NW 43RD TER	NW 46TH ST	69.7	88
NW102NDAVE	240	NW 66TH ST	Dead End	N/A	100
NW102NDAVE	250	NW 28TH TER	191 ft N of NW 28TH TER	93	96
NW102NDAVE	260	NW 52ND ST	NW 56TH TER	67.3	92
NW102NDAVE	270	NW 102ND AVE	414 ft N of NW 102ND AVE	74	99
NW102NDAVE	280	455 ft S of NW 48TH STREET CIR	NW 48TH STREET CIR	74	99
NW102NDAVE	290	NW 49TH LN	NW 52ND ST	74	99
NW102NDPL	10	NW 21ST ST	NW 25TH ST	56.6	82
NW104THAVE	10	NW 36TH ST	NW 37TH TER	81.9	76
NW104THAVE	20	NW 37TH TER	NW 41ST ST	78.5	83
NW104THAVE	30	NW 58TH ST	Dead End	84.1	71
NW104THAVE	40	NW 33RD ST	NW 36TH ST	90.3	96
NW104THCT	10	NW 26TH ST	NW 28TH ST	70.5	92
NW105THAVE	10	NW 31ST TER	NW 33RD ST	88.8	92
NW105THAVE	20	NW 26TH ST	NW 27TH ST	90.5	96
NW105THAVE	30	NW 27TH ST	NW 28TH ST	77.8	97
NW108THAVE	10	NW 27TH ST	NW 29TH ST	90.3	100
NW108THAVE	20	NW 29TH ST	NW 30TH ST	100	88
NW108THAVE	30	NW 30TH ST	NW 33RD ST	100	100
NW109THAVE	10	681 ft S of NW 90TH ST	NW 90TH ST	78	83
NW109THAVE	20	NW 43RD LN	NW 48TH LN	86	93
NW109THAVE	30	NW 50TH ST	NW 51ST LN	78.5	98
NW109THAVE	40	NW 51ST LN	NW 53RD LN	78.5	98
NW109THAVE	50	NW 30TH ST	NW 33RD ST	100	100
NW109THAVE	60	NW 29TH ST	NW 109TH AVE	44.9	100
NW109THAVE	70	NW 53RD LN	NW 58TH ST	72.1	76
NW109THAVE	80	NW 27TH ST	NW 29TH ST	70.7	100
NW109THAVE	90	NW 109TH AVE	NW 30TH ST	44.9	100
NW109THAVE	100	NW 87TH ST	320 ft N of NW 87TH ST	78	96
NW109THAVE	110	NW 48TH LN	NW 50TH ST	86	98
NW109THAVE	120	NW 41ST ST	NW 43RD LN	N/A	97
NW109THAVE	130	NW 86TH ST	NW 87TH ST	70.2	99
NW110THAVE	10	NW 29TH ST	Dead End	100	85
NW112THAVE	10	NW 74TH ST	NW 74TH TER	100	91
NW112THAVE	20	NW 53RD LN	NW 58TH ST	86	96
NW112THAVE	30	NW 25TH ST	NW 27TH ST	73.2	100
NW112THAVE	40	NW 74TH TER	NW 75TH TER	100	91
NW112THAVE	50	NW 27TH ST	NW 29TH ST	46.9	100
NW112THAVE	60	NW 75TH LN	NW 77TH TER	100	82
NW112THAVE	70	NW 77TH TER	NW 78TH ST	100	78
NW112THAVE	80	NW 80TH LN	NW 82ND ST	100	83
NW112THAVE	90	NW 46TH LN	NW 48TH TER	90.3	99
NW112THAVE	100	NW 75TH TER	NW 75TH LN	100	81
NW112THAVE	110	NW 41ST ST	NW 42ND LN	69.3	100
NW112THAVE	120	NW 52ND ST	NW 53RD LN	87.1	93
NW112THAVE	130	NW 79TH LN	NW 80TH LN	100	95
NW112THAVE	140	NW 78TH ST	NW 78TH LN	100	83
NW112THAVE	150	NW 42ND LN	NW 43RD LN	69.3	98
NW112THAVE	160	NW 43RD LN	NW 46TH LN	69.3	96
NW112THAVE	170	NW 84TH ST	NW 86TH ST	N/A	100
NW112THAVE	180	301 ft S of NW 84TH ST	NW 84TH ST	N/A	99
NW112THAVE	190	NW 29TH ST	NW 30TH ST	100	100
NW112THAVE	200	NW 86TH ST	NW 87TH ST	N/A	100
NW112THAVE	210	NW 78TH LN	NW 79TH LN	100	72
NW112THAVE	220	NW 82ND ST	NW 112TH AVE	100	93
NW112THAVE	230	NW 50TH ST	NW 52ND ST	87.1	95
NW112THAVE	240	NW 48TH TER	NW 50TH ST	90.3	91
NW112THAVE	250	NW 112TH AVE	286 ft NW of NW 112TH AVE	N/A	100
NW112THAVE	260	NW 30TH ST	NW 33RD ST	90.3	100
NW112THAVE	270	NW 33RD ST	NW 34TH ST	90.3	77
NW112THAVE	280	NW 87TH ST	NW 90TH ST	100	99
NW113THCT	10	NW 34TH ST	NW 36TH TER	N/A	99
NW114THAVE	10	NW 73RD TER	NW 74TH ST	90.3	59
NW114THAVE	20	NW 114TH AVE	NW 60TH ST	87.1	65
NW114THAVE	30	NW 77TH LN	NW 78TH ST	75.2	65
NW114THAVE	40	NW 62ND TER	NW 114TH AVE	100	65
NW114THAVE	50	NW 34TH ST	NW 36TH TER	87.1	98
NW114THAVE	60	NW 41ST ST	NW 42ND TER	72	100
NW114THAVE	70	NW 42ND TER	NW 43RD TER	72	100
NW114THAVE	80	NW 48TH TER	NW 114TH AVE	83.1	98
NW114THAVE	90	NW 39TH ST	NW 41ST ST	90.3	92
NW114THAVE	100	297 ft S of NW 55TH ST	NW 55TH ST	100	88
NW114THAVE	110	NW 36TH TER	NW 39TH ST	83.1	97
NW114THAVE	120	NW 72ND ST	NW 73RD TER	90.3	70
NW114THAVE	130	NW 58TH ST	NW 114TH AVE	87.1	100
NW114THAVE	140	NW 78TH ST	496 ft N of NW 78TH ST	100	65

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NW114THAVE	150	NW 114TH AVE	NW 114TH AVE	100	70
NW114THAVE	160	NW 57TH ST	NW 58TH ST	87.3	76
NW114THAVE	170	NW 43RD TER	NW 44TH ST	72	83
NW114THAVE	180	NW 114TH AVE	NW 66TH ST	100	82
NW114THAVE	190	NW 44TH ST	NW 47TH ST	83.1	98
NW114THAVE	200	NW 47TH ST	NW 48TH TER	83.1	94
NW114THAVE	210	NW 69TH TER	NW 72ND ST	90.3	63
NW114THAVE	220	NW 67TH TER	NW 68TH TER	89.2	100
NW114THAVE	230	NW 68TH TER	NW 69TH TER	90.3	64
NW114THAVE	240	NW 66TH ST	NW 67TH TER	89.2	65
NW114THAVE	250	NW 55TH ST	NW 57TH ST	87.3	100
NW114THAVE	260	NW 114TH AVE	NW 50TH ST	83.1	93
NW114THAVE	270	NW 51ST TER	928 ft N of NW 51ST TER	100	96
NW114THAVE	280	NW 75TH LN	NW 77TH LN	75.2	79
NW114THAVE	300	NW 74TH ST	NW 75TH LN	75.2	68
NW114THAVE	310	NW 60TH ST	NW 62ND TER	87.1	69
NW114THAVE	320	NW 50TH ST	NW 51ST TER	100	98
NW115THAVE	10	NW 34TH ST	NW 39TH ST	62.1	96
NW115THAVE	20	NW 39TH ST	NW 41ST ST	62.1	97
NW117THAVE	10	FLORIDA TPKE RAMP	NW 50TH ST	84.4	100
NW117THAVE	20	NW 117TH AVE	NW 58TH ST	17	63
NW117THAVE	30	NW 50TH ST	NW 117TH AVE	17	84
NW117THAVE	40	NW 34TH ST	Dead End	46.9	63
NW13THST	10	NW 13TH ST	NW 97TH AVE	67.3	100
NW13THST	20	NW 13TH ST	NW 13TH ST	67.3	100
NW13THST	30	NW 13TH ST	NW 13TH ST	67.3	100
NW13THST	40	NW 93RD CT	NW 13TH ST	67.3	100
NW13THTER	10	NW 89TH CT	Dead End	88.8	89
NW13THTER	20	NW 87TH AVE	NW 88TH AVE	73	76
NW13THTER	30	NW 88TH AVE	NW 89TH CT	62.5	76
NW14THST	10	NW 98TH CT	NW 100TH AVE	90.3	99
NW14THST	20	NW 82ND AVE	Dead End	50.9	85
NW14THST	30	NW 79TH AVE	NW 82ND AVE	77.7	84
NW15THST	10	NW 78TH AVE	NW 79TH AVE	77.9	63
NW15THST	20	NW 89TH CT	Dead End	100	82
NW15THST	30	NW 87TH AVE	NW 88TH AVE	71.4	76
NW15THST	40	NW 88TH AVE	NW 89TH CT	81.1	94
NW15THTER	10	NW 100TH AVE	NW 102ND AVE	93	100
NW17THST	10	NW 98TH CT	NW 19TH ST	86	92
NW17THST	20	NW 97TH AVE	NW 98TH CT	88	100
NW17THST	30	NW 84TH AVE	NW 87TH AVE	69.3	83
NW17THST	40	NW 82ND AVE	783 ft W of NW 82ND AVE	76	90
NW18THTER	10	NW 88TH CT	NW 89TH PL	64.9	91
NW18THTER	20	NW 87TH AVE	NW 88TH CT	63.9	90
NW19THST	10	NW 17TH ST	NW 102ND AVE	86	98
NW19THST	20	NW 102ND AVE	NW 107TH AVE	86	95
NW20THST	10	NW 88TH CT	NW 89TH PL	77.7	70
NW215TST	10	NW 102ND AVE	NW 102ND PL	93	87
NW215TST	20	392 ft E of NW 215T ST	NW 215T ST		84
NW215TST	30	NW 79TH AVE	NW 215T ST	66.5	60
NW215TST	40	NW 102ND PL	NW 215T ST	73.3	95
NW215TST	50	NW 215T ST	NW 82ND AVE	66.5	51
NW215TST	60	NW 215T ST	436 ft W of NW 215T ST		100
NW215TST	70	NW 215T ST	NW 215T ST		86
NW215TST	80	NW 99TH AVE	NW 215T ST	73	82
NW215TST	90	NW 215T ST	NW 102ND AVE	73	82
NW215TST	100	NW 215T ST	NW 215T ST		86
NW215TST	110	NW 82ND AVE	Dead End	81.6	95
NW215TST	120	NW 215T ST	NW 107TH AVE		86
NW215TTER	10	NW 86TH AVE	NW 87TH AVE	77.1	80
NW215TTER	20	NW 87TH AVE	NW 88TH CT	47	78
NW23RDST	10	NW 88TH CT	NW 89TH PL	86	76
NW23RDST	20	NW 84TH AVE	NW 86TH AVE	76	86
NW24THTER	10	NW 25TH ST	NW 89TH PL	40.3	83
NW25THTER	10	NW 99TH AVE	NW 100TH AVE	80.2	68
NW25THTER	20	NW 98TH AVE	NW 99TH AVE	87	96
NW26THST	10	NW 104TH CT	NW 105TH AVE	88.8	91
NW26THST	20	NW 87TH CT	NW 89TH CT	88.8	86
NW26THST	30	NW 100TH AVE	NW 102ND AVE	87	84
NW26THST	40	NW 99TH AVE	NW 100TH AVE	89.2	85
NW26THST	50	NW 98TH AVE	NW 99TH AVE	84.3	90
NW26THST	60	NW 97TH AVE	NW 98TH AVE		87
NW27THST	10	NW 109TH AVE	NW 112TH AVE	86	97
NW27THST	20	NW 97TH AVE	NW 98TH AVE	89.1	100
NW27THST	30	NW 82ND AVE	NW 84TH AVE	42.2	78
NW27THST	40	NW 87TH CT	NW 89TH CT	84.4	81
NW27THST	50	NW 99TH AVE	NW 100TH AVE	93	91
NW27THST	60	NW 84TH AVE	NW 87TH AVE	54.6	78
NW27THST	70	NW 87TH AVE	NW 87TH CT	61.1	84
NW27THST	80	NW 98TH AVE	NW 99TH AVE	93	86
NW27THST	90	NW 108TH AVE	NW 109TH AVE	41.4	100
NW27THST	100	NW 107TH AVE	NW 108TH AVE	77.5	100
NW27THST	110	NW 105TH AVE	NW 107TH AVE	89.2	100
NW27THST	120	NW 100TH AVE	NW 102ND AVE	93	83
NW27THTER	10	NW 100TH AVE	NW 102ND AVE	73.2	100
NW27THTER	20	NW 99TH AVE	NW 100TH AVE	71.8	100
NW27THTER	30	NW 98TH AVE	NW 99TH AVE	80.2	100
NW27THTER	40	NW 97TH AVE	NW 98TH AVE	48.2	100
NW28THST	10	NW 104TH CT	NW 105TH AVE	84.4	97
NW28THTER	10	NW 97TH AVE	NW 98TH AVE	88.8	100
NW28THTER	20	NW 99TH AVE	NW 100TH AVE	87.4	100
NW28THTER	30	NW 98TH AVE	NW 99TH AVE	84.4	80
NW28THTER	40	NW 100TH AVE	NW 102ND AVE	87.4	100
NW29THST	10	Dead End	NW 107TH AVE		100
NW29THST	20	NW 79TH AVE	NW 82ND AVE	63.9	77
NW29THST	30	NW 78TH AVE	NW 79TH AVE	69.5	86

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NW29THST	40	NW 109TH AVE	NW 110TH AVE	87.1	95
NW29THST	50	NW 108TH AVE	NW 109TH AVE	75.8	91
NW29THST	60	NW 110TH AVE	NW 112TH AVE	83.1	87
NW29THST	70	Dead End	NW 92ND AVE	100	100
NW29THST	80	NW 84TH AVE	NW 87TH AVE	72	92
NW29THST	90	NW 77TH CT	NW 78TH AVE	69.5	80
NW29THTER	10	Dead End	NW 107TH AVE	51.9	82
NW30THST	10	NW 108TH AVE	NW 109TH AVE	88.8	99
NW30THST	20	NW 109TH AVE	NW 112TH AVE	90.3	99
NW30THTER	10	NW 82ND AVE	NW 84TH AVE	25.7	71
NW30THTER	20	NW 84TH AVE	NW 85TH AVE	46.7	84
NW30THTER	30	NW 85TH AVE	NW 87TH AVE		78
NW31STST	10	NW 79TH AVE	NW 82ND AVE	34.7	91
NW31STST	20	Dead End	NW 31ST TER		95
NW31STTER	10	NW 105TH AVE	NW 107TH AVE		93
NW31STTER	20	NW 31ST ST	NW 105TH AVE		100
NW33RDST	10	TORREMOLINOS AVE	NW 104TH AVE	78	81
NW33RDST	20	NW 84TH AVE	849 ft W of NW 84TH AVE	89.2	75
NW33RDST	30	NW 89TH CT	NW 91ST AVE	91.2	91
NW33RDST	40	NW 91ST AVE	Dead End	84.4	93
NW33RDST	50	NW 87TH AVE	NW 89TH CT	91.2	99
NW33RDST	60	633 ft E of NW 87TH AVE	NW 87TH AVE		83
NW33RDST	70	NW 82ND AVE	655 ft W of NW 82ND AVE	89.2	66
NW33RDST	80	NW 97TH AVE	NW 98TH CT		98
NW33RDST	90	575 ft E of NW 84TH AVE	NW 84TH AVE	89.2	67
NW33RDST	100	NW 104TH AVE	NW 105TH AVE	51	100
NW33RDST	110	NW 79TH AVE	NW 82ND AVE	34.7	73
NW33RDST	120	NW 107TH AVE	NW 108TH AVE	88.4	92
NW33RDST	130	NW 109TH AVE	NW 112TH AVE	89.1	93
NW33RDST	140	NW 108TH AVE	NW 109TH AVE	89.1	95
NW33RDST	150	337 ft E of NW 33RD ST	NW 33RD ST		87
NW33RDST	160	NW 105TH AVE	NW 107TH AVE	90.3	93
NW33RDST	170	NW 33RD ST	TORREMOLINOS AVE		86
NW33RDST	180	NW 98TH CT	1327 ft W of NW 98TH CT		84
NW34THST	10	NW 115TH AVE	NW 117TH AVE	93	93
NW34THST	20	NW 114TH AVE	NW 115TH AVE	68.3	100
NW34THST	30	NW 113TH CT	NW 114TH AVE	90.3	100
NW34THST	40	NW 112TH AVE	NW 113TH CT	81.6	100
NW35THLN	10	NW 87TH AVE	NW 89TH CT	91.2	79
NW36THST	10	NW 104TH AVE	NW 107TH AVE	89.2	100
NW36THTER	10	NW 113TH CT	NW 114TH AVE	88.8	90
NW37THST	10	NW 78TH AVE	NW 79TH AVE	63.9	52
NW37THST	20	NW 77TH CT	NW 78TH AVE	32.5	50
NW37THTER	10	NW 104TH AVE	NW 107TH AVE	86	99
NW38THST	10	NW 78TH AVE	NW 79TH AVE	57.3	36
NW38THST	20	NW 40TH STREET RD	NW 97TH AVE	68.1	66
NW40THST	10	NW 38TH ST	NW 40TH STREET RD	88.3	64
NW40THST	20	NW 40TH STREET RD	NW 97TH AVE		73
NW41STST	10	NW 81ST AVE	NW 82ND AVE	25.7	14
NW41STST	20	NW 79TH AVE	NW 81ST AVE	25.7	55
NW41STST	40	NW 41ST ST	NW 87TH AVE		63
NW41STST	80	NW 82ND AVE	NW 41ST ST	27.7	45
NW50THST	10	NW 112TH AVE	NW 114TH AVE	64.1	81
NW50THST	20	NW 114TH CT	NW 115TH CT		100
NW50THST	30	NW 107TH AVE	NW 109TH AVE	72.1	90
NW50THST	40	NW 109TH AVE	NW 112TH AVE	79	100
NW50THST	50	NW 115TH CT	NW 117TH AVE	80.3	98
NW50THST	60	1218 ft E of NW 79TH AVE	NW 79TH AVE	9.1	64
NW50THST	70	NW 114TH AVE	NW 114TH CT	80.3	97
NW52NDST	20	NW 104TH AVE	NW 104TH CT		91
NW52NDST	30	NW 84TH AVE	NW 84TH AVE		97
NW52NDST	40	NW 84TH AVE	175 ft W of NW 84TH AVE	39.2	96
NW52NDST	50	521 FT E OF NW 87TH AVE	367 FT W OF NW 84TH AVE		93
NW52NDST	70	NW 77TH CT	NW 78TH AVE	34.3	73
NW52NDST	80	NW 105TH CT	NW 106TH AVE		99
NW52NDST	90	NW 52ND ST	NW 102ND AVE	72.6	94
NW52NDST	110	NW 104TH CT	NW 105TH CT		99
NW52NDST	120	269 ft E of NW 87TH AVE	NW 87TH AVE		80
NW52NDST	130	635 FT E OF NW 87TH AVE	269 FT W OF NW 84TH AVE		100
NW52NDST	140	NW 102ND AVE	NW 102ND PL	88.8	99
NW52NDST	150	NW 78TH AVE	NW 79TH AVE	26	73
NW52NDST	160	NW 106TH CT	NW 107TH AVE		89
NW52NDST	170	NW 106TH AVE	NW 106TH CT		74
NW52NDST	200	NW 99TH AVE	NW 52ND ST		93
NW52NDST	210	NW 97TH AVE	NW 99TH AVE		83
NW52NDST	220	NW 102ND PL	NW 104TH AVE		98
NW52NDST	230	269 FT E OF NW 87TH AVE	521 FT E OF NW 87TH AVE		74
NW52NTER	10	NW 53RD ST	NW 52ND TER		100
NW52NTER	20	NW 52ND TER	NW 53RD ST		81
NW53RDST	20	NW 78TH AVE	NW 79TH AVE	48.8	73
NW53RDST	80	NW 84TH AVE	277 ft W of NW 84TH AVE	51	93
NW53RDST	90	NW 52ND TER	NW 52ND TER	63.4	88
NW53RDST	100	NW 52ND TER	NW 84TH AVE	35.2	77
NW53RDST	110	NW 79TH AVE	NW 52ND TER	56	92
NW53RDST	120	527 FT E OF NW 87TH AVE	277 FT W OF NW 84TH AVE		82
NW53RDST	130	279 ft E of NW 87TH AVE	492 FT W OF 84TH AVE		86
NW53RDST	140	279 ft E of NW 87TH AVE	NW 87TH AVE		66
NW53RDST	160	NW 77TH CT	NW 78TH AVE	80.4	100
NW53RDTER	10	NW 84TH AVE	NW 87TH AVE	64.1	83
NW54THST	10	NW 82ND AVE	NW 84TH AVE	0	58
NW54THST	20	NW 78TH AVE	NW 79TH AVE	54.4	90
NW54THST	30	NW 77TH CT	NW 78TH AVE	69.7	88
NW54THST	40	NW 84TH AVE	NW 87TH AVE	38.8	67
NW54THST	50	NW 79TH AVE	NW 82ND AVE	6.2	72
NW55THST	10	NW 77TH CT	NW 78TH AVE	78	90
NW55THST	20	NW 78TH AVE	NW 79TH AVE	52.3	89

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NW56THST	10	NW 82ND AVE	NW 84TH AVE	30.2	84
NW56THST	20	NW 79TH AVE	NW 82ND AVE	40.6	74
NW56THST	30	NW 84TH AVE	NW 87TH AVE	33.6	64
NW56THST	40	NW 78TH AVE	NW 79TH AVE	61.1	85
NW56THST	50	NW 77TH CT	NW 78TH AVE	31.5	95
NW57THST	10	Dead End	NW 78TH AVE	83.5	100
NW57THST	20	NW 78TH AVE	NW 79TH AVE	81.1	100
NW66THST	10	1298 ft E of NW 107TH AVE	NW 107TH AVE	n/a	100
NW66THST	20	NW 97TH AVE	2592 ft W of NW 102TH AVE	n/a	93
NW77THCT	10	NW 53RD ST	NW 54TH ST	19	62
NW77THCT	20	NW 52ND ST	NW 53RD ST	11.9	64
NW77THCT	30	NW 54TH ST	NW 55TH ST	50.6	66
NW77THCT	40	NW 55TH ST	NW 56TH ST	72.6	75
NW77THCT	50	NW 29TH ST	NW 32ND ST	36.8	71
NW78THAVE	10	NW 55TH ST	NW 56TH ST	64.1	84
NW78THAVE	20	NW 56TH ST	NW 57TH ST	69.8	100
NW78THAVE	30	NW 12TH ST	NW 15TH ST	54.5	68
NW78THAVE	40	NW 52ND ST	NW 53RD ST	69.7	98
NW78THAVE	50	NW 53RD ST	NW 54TH ST		100
NW78THAVE	60	NW 57TH ST	Dead End		100
NW78THAVE	70	NW 54TH ST	NW 55TH ST	86	89
NW78THAVE	80	NW 37TH ST	NW 38TH ST	13.2	51
NW79THAVE	10	NW 29TH ST	NW 31ST ST	71.9	100
NW79THAVE	20	NW 53RD ST	NW 54TH ST	82.2	100
NW79THAVE	30	NW 25TH ST	NW 29TH ST	62.5	100
NW79THAVE	40	NW 41ST ST	262 ft N of NW 41ST ST	80.7	99
NW79THAVE	50	NW 46TH ST	271FT S OF NW 46TH ST	80.7	99
NW79THAVE	60	NW 46TH ST	60 FT N OF NW 46TH ST	62.5	100
NW79THAVE	70	NW 41ST ST	NW 41ST ST		100
NW79THAVE	80	NW 54TH ST	NW 55TH ST	85.4	100
NW79THAVE	90	NW 55TH ST	NW 56TH ST	85.4	100
NW79THAVE	100	262 FT N OF NW 41ST ST	NW 43RD ST	80.7	100
NW79THAVE	110	NW 57TH ST	NW 58TH ST	63.4	100
NW79THAVE	120	NW 31ST ST	NW 32ND ST	76.3	100
NW79THAVE	130	NW 33RD ST	NW 34TH ST	42.9	100
NW79THAVE	140	NW 56TH ST	NW 57TH ST	77.9	100
NW79THAVE	150	NW 52ND ST	NW 53RD ST	89.2	99
NW79THAVE	160	NW 14TH ST	NW 15TH ST	77.9	62
NW79THAVE	170	NW 15TH ST	NW 17TH ST	46.4	65
NW79THAVE	180	NW 32ND ST	NW 33RD ST	76.3	100
NW79THAVE	190	NW 47TH ST	NW 48TH ST	62.5	99
NW79THAVE	200	NW 48TH ST	NW 48TH ST	62.5	100
NW79THAVE	210	NW 34TH ST	NW 79TH AVE		100
NW79THAVE	220	309 ft S of NW 51ST ST	NW 51ST ST		100
NW79THAVE	230	NW 48TH ST	NW 50TH ST	62.5	100
NW79THAVE	240	NW 51ST ST	NW 52ND ST	77.7	100
NW79THAVE	250	179 ft S of NW 79TH AVE	NW 79TH AVE		99
NW79THAVE	260	NW 39TH ST	NW 41ST ST	69.3	100
NW79THAVE	270	NW 36TH ST	NW 39TH ST		96
NW79THAVE	280	NW 38TH ST	NW 36TH ST	72	94
NW79THAVE	290	NW 79TH AVE	NW 37TH ST	42.9	100
NW79THAVE	300	NW 37TH ST	NW 38TH ST	72	99
NW79THAVE	310	NW 50TH ST	169 ft N of NW 50TH ST	77.7	100
NW79THAVE	320	NW 79TH AVE	159 ft N of NW 79TH AVE		100
NW79THAVE	330	386 ft S of NW 47TH ST	NW 47TH ST		100
NW79THAVE	340	NW 17TH ST	NW 79TH AVE	46.4	71
NW79THAVE	350	NW 79TH AVE	NW 21ST ST	46.4	79
NW82NDAVE	10	NW 54TH ST	NW 56TH ST	87.9	77
NW82NDAVE	20	NW 82ND AVE	NW 25TH ST		85
NW82NDAVE	30	NW 56TH ST	NW 58TH ST	57.6	90
NW82NDAVE	40	NW 33RD ST	779 ft N of NW 33RD ST	71.1	55
NW82NDAVE	50	NW 27TH ST	NW 29TH ST	69.7	76
NW82NDAVE	60	NW 17TH ST	NW 82ND AVE	72.9	92
NW82NDAVE	70	NW 12TH ST	NW 82ND AVE		99
NW82NDAVE	80	NW 25TH ST	NW 27TH ST	83.5	82
NW82NDAVE	90	NW 14TH ST	NW 17TH ST	46.8	80
NW82NDAVE	100	NW 82ND AVE	NW 14TH ST	52.7	88
NW82NDAVE	110	NW 31ST ST	NW 33RD ST	54.8	93
NW82NDAVE	120	NW 36TH ST	NW 41ST ST	72	70
NW82NDAVE	130	NW 21ST ST	NW 82ND AVE	38.2	100
NW82NDAVE	140	547 ft S of NW 37TH ST	NW 37TH ST		63
NW82NDAVE	150	NW 29TH ST	NW 30TH TER	72	75
NW82NDAVE	160	NW 82ND AVE	NW 82ND AVE		89
NW82NDAVE	170	NW 30TH TER	NW 31ST ST	80.2	99
NW82NDAVE	180	NW 82ND AVE	NW 21ST ST	72.9	81
NW82NDAVE	190	NW 37TH ST	NW 36TH ST		91
NW82NDAVE	200	NW 82ND AVE	NW 82ND AVE		100
NW82NDST	10	NW 114TH PATH	NW 117TH AVE		93
NW82NDST	20	NW 112TH AVE	NW 114TH PATH		94
NW82NDST	30	NW 107TH AVE	NW 112TH AVE		98
NW84THAVE	10	NW 27TH ST	NW 29TH ST	68.1	68
NW84THAVE	20	NW 30TH TER	NW 33RD ST	54.5	65
NW84THAVE	30	NW 23RD ST	NW 25TH ST	44.9	93
NW84THAVE	40	NW 84TH AVE	NW 17TH ST	77.7	88
NW84THAVE	50	NW 84TH AVE	NW 53RD ST	79	87
NW84THAVE	60	NW 54TH ST	NW 56TH ST	88.7	100
NW84THAVE	70	NW 17TH ST	NW 19TH ST	84.4	86
NW84THAVE	80	NW 84TH AVE	NW 84TH AVE		88
NW84THAVE	90	NW 84TH AVE	NW 23RD ST	76.3	87
NW84THAVE	100	NW 52ND ST	NW 84TH AVE	79	100
NW84THAVE	110	NW 56TH ST	NW 58TH ST	72.3	97
NW84THAVE	120	NW 19TH ST	NW 84TH AVE		100
NW84THAVE	130	NW 13TH TER	NW 84TH AVE		92
NW84THAVE	140	NW 29TH ST	NW 30TH TER	67.4	68
NW84THAVE	150	NW 27TH ST	Dead End	68.1	72
NW84THAVE	160	NW 12TH ST	NW 13TH TER		91

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NW84THAVE	170	NW 53RD ST	NW 53RD TER	77.1	65
NW86THAVE	10	NW 21ST TER	NW 23RD ST	76.3	86
NW86THST	10	NW 110TH PL	NW 111TH CT		80
NW86THST	20	NW 111TH CT	NW 112TH AVE		100
NW86THST	30	NW 109TH AVE	NW 110TH AVE		92
NW86THST	40	NW 108TH AVE	NW 108TH CT		98
NW86THST	50	NW 110TH AVE	NW 110TH PL		83
NW86THST	60	NW 108TH CT	NW 109TH AVE		100
NW86THST	70	NW 107TH AVE	NW 108TH AVE		100
NW87THCT	10	NW 26TH ST	NW 27TH ST	80.2	86
NW88THAVE	10	NW 15TH ST	Dead End	78	93
NW88THAVE	20	NW 13TH TER	NW 15TH ST	75.1	82
NW88THCT	10	NW 18TH TER	NW 20TH ST	76.2	71
NW88THCT	20	NW 20TH ST	NW 21ST TER	90	85
NW88THCT	30	NW 21ST TER	NW 23RD ST	68.7	63
NW89THCT	10	NW 15TH ST	Dead End	45.4	65
NW89THCT	20	NW 33RD ST	NW 35TH LN	100	84
NW89THCT	30	NW 26TH ST	NW 27TH ST	77.9	82
NW89THCT	40	NW 25TH ST	NW 26TH ST	89.2	94
NW89THCT	50	NW 13TH TER	NW 15TH ST	70.3	89
NW89THCT	60	NW 12TH ST	NW 13TH TER	66.8	87
NW89THPL	10	NW 18TH TER	NW 20TH ST	88.4	94
NW89THPL	20	NW 23RD ST	NW 25TH ST	79.8	92
NW89THPL	30	NW 20TH ST	NW 23RD ST	74.6	92
NW89THPL	40	NW 25TH ST	NW 25TH ST	57.7	73
NW91STAVE	10	NW 33RD ST	Dead End	100	87
NW92NDAVE	10	Dead End	NW 58TH ST		70
NW92NDAVE	20	320 ft S of NW 29TH ST	NW 29TH ST	70.1	100
NW92NDAVE	30	NW 25TH ST	319 ft N of NW 25TH ST		87
NW92NDAVE	40	618 FT S OF NW 29TH ST	319 FT N OF NW 25TH ST		90
NW92NDAVE	50	320 FT S OF NW 29TH ST	618 FT S OF NW 29TH ST		86
NW93RDCT	10	NW 93RD CT	NW 13TH ST	38.3	100
NW93RDCT	20	NW 12TH ST	NW 93RD CT	38.3	100
NW98THAVE	10	NW 28TH TER	Dead End		100
NW98THAVE	20	NW 26TH ST	NW 27TH ST	80.2	100
NW98THAVE	30	NW 27TH TER	NW 28TH TER	93	100
NW98THAVE	40	NW 25TH TER	NW 26TH ST	69.4	100
NW98THAVE	50	NW 27TH ST	NW 27TH TER	83.5	100
NW98THCT	10	NW 14TH ST	NW 17TH ST	88.8	93
NW98THCT	20	NW 12TH ST	NW 14TH ST	88.8	100
NW99THAVE	10	NW 26TH ST	NW 27TH ST	83.1	100
NW99THAVE	20	NW 99TH AVE	NW 60TH ST		62
NW99THAVE	30	NW 27TH TER	NW 28TH TER	56	100
NW99THAVE	40	NW 28TH TER	Dead End		100
NW99THAVE	50	NW 21ST ST	NW 25TH ST	76.1	68
NW99THAVE	60	NW 25TH TER	NW 26TH ST	69.4	100
NW99THAVE	70	NW 27TH ST	NW 27TH TER	73.5	100
NW99THAVE	80	NW 25TH ST	NW 25TH TER	65.7	100
NW99THAVE	90	NW 58TH ST	NW 99TH AVE		88



APPENDIX C

RECOMMENDED PROJECTS



RECOMMENDED PROJECTS FOR CRITICAL PCI METHOD

Year	NetworkID	BranchID	SectionID	True Area	Area Units	PCI Before	Cost	Delay Penalty
1/9/2012	CITYROAD	NW102NDAVE	200	3,656.00	SqFt	84.71	\$5,484.00	3.00%
1/9/2012	CITYROAD	NW104THAVE	10	11,007.00	SqFt	75.76	\$18,843.00	6.97%
1/9/2012	CITYROAD	NW112THAVE	70	5,053.00	SqFt	77.71	\$8,158.00	5.23%
1/9/2012	CITYROAD	NW112THAVE	100	5,500.00	SqFt	80.44	\$8,250.00	6.98%
1/9/2012	CITYROAD	NW13THTER	10	6,157.00	SqFt	88.25	\$12,314.00	3.00%
1/9/2012	CITYROAD	NW13THTER	20	22,573.00	SqFt	75.32	\$45,146.00	12.24%
1/9/2012	CITYROAD	NW13THTER	30	15,911.00	SqFt	75.32	\$31,823.00	12.24%
1/9/2012	CITYROAD	NW14THST	20	22,757.00	SqFt	84.24	\$45,514.00	3.00%
1/9/2012	CITYROAD	NW17THST	30	44,154.00	SqFt	82.32	\$88,309.00	3.00%
1/9/2012	CITYROAD	NW18THTER	10	41,619.00	SqFt	90.22	\$83,238.00	3.00%
1/9/2012	CITYROAD	NW18THTER	20	14,976.00	SqFt	89.23	\$29,952.00	3.00%
1/9/2012	CITYROAD	NW215TST	30	20,351.00	SqFt	57.67	\$40,702.00	3.00%
1/9/2012	CITYROAD	NW215TST	50	26,289.00	SqFt	47.84	\$52,578.00	120.96%
1/9/2012	CITYROAD	NW24THTER	10	21,529.00	SqFt	82.52	\$32,294.00	3.00%
1/9/2012	CITYROAD	NW26THST	60	12,994.00	SqFt	86.17	\$19,492.00	3.00%
1/9/2012	CITYROAD	NW27THST	60	33,710.00	SqFt	77.71	\$54,426.00	5.23%
1/9/2012	CITYROAD	NW29THST	20	39,410.00	SqFt	76.87	\$78,819.00	3.78%
1/9/2012	CITYROAD	NW29THST	30	17,267.00	SqFt	85.2	\$34,534.00	3.00%
1/9/2012	CITYROAD	NW29THST	60	21,023.00	SqFt	86.31	\$31,535.00	3.00%
1/9/2012	CITYROAD	NW29THST	90	18,984.00	SqFt	79.51	\$37,969.00	7.53%
1/9/2012	CITYROAD	NW30THTER	30	17,664.00	SqFt	77.71	\$28,519.00	5.23%
1/9/2012	CITYROAD	NW315TST	10	29,029.00	SqFt	90.12	\$58,059.00	3.00%
1/9/2012	CITYROAD	NW33RDST	10	21,187.00	SqFt	80.49	\$31,781.00	8.49%
1/9/2012	CITYROAD	NW33RDST	180	39,797.00	SqFt	83.69	\$59,695.00	3.00%
1/9/2012	CITYROAD	NW37THST	10	13,206.00	SqFt	48.93	\$26,413.00	87.82%
1/9/2012	CITYROAD	NW37THST	20	5,597.00	SqFt	46.76	\$11,193.00	153.64%
1/9/2012	CITYROAD	NW38THST	10	13,274.00	SqFt	31.62	\$26,548.00	16.01%
1/9/2012	CITYROAD	NW40THST	20	10,592.00	SqFt	72.19	\$20,024.00	8.97%
1/9/2012	CITYROAD	NW50THST	10	26,440.00	SqFt	80.44	\$39,660.00	6.98%
1/9/2012	CITYROAD	NW52NDST	70	10,753.00	SqFt	72.19	\$20,328.00	8.97%
1/9/2012	CITYROAD	NW52NDST	150	13,431.00	SqFt	72.19	\$25,390.00	8.97%
1/9/2012	CITYROAD	NW52NDST	160	6,883.00	SqFt	88.54	\$10,325.00	3.00%
1/9/2012	CITYROAD	NW52NDST	210	24,122.00	SqFt	82.65	\$36,183.00	3.00%
1/9/2012	CITYROAD	NW52NDST	230	6,039.00	SqFt	73.35	\$11,066.00	11.63%
1/9/2012	CITYROAD	NW52NDTER	20	26,968.00	SqFt	80.44	\$40,452.00	6.98%
1/9/2012	CITYROAD	NW53RDST	20	13,540.00	SqFt	72.19	\$25,598.00	8.97%
1/9/2012	CITYROAD	NW53RDST	100	11,401.00	SqFt	76.87	\$18,885.00	3.78%
1/9/2012	CITYROAD	NW53RDST	130	4,967.00	SqFt	85.2	\$7,450.00	3.00%
1/9/2012	CITYROAD	NW53RDTER	10	24,809.00	SqFt	82.32	\$37,213.00	3.00%
1/9/2012	CITYROAD	NW54THST	50	26,276.00	SqFt	71.04	\$51,186.00	5.75%
1/9/2012	CITYROAD	NW56THST	10	29,212.00	SqFt	83.28	\$43,818.00	3.00%
1/9/2012	CITYROAD	NW56THST	20	28,912.00	SqFt	73.35	\$52,981.00	11.63%
1/9/2012	CITYROAD	NW56THST	40	14,634.00	SqFt	84.24	\$21,951.00	3.00%
1/9/2012	CITYROAD	NW77THCT	40	6,953.00	SqFt	74.53	\$12,331.00	9.68%
1/9/2012	CITYROAD	NW77THCT	50	19,197.00	SqFt	69.89	\$38,395.00	3.00%
1/9/2012	CITYROAD	NW78THAVE	10	7,199.00	SqFt	83.28	\$10,798.00	3.00%
1/9/2012	CITYROAD	NW78THAVE	30	59,595.00	SqFt	66.32	\$119,189.00	3.00%
1/9/2012	CITYROAD	NW82NDAVE	10	15,939.00	SqFt	76.87	\$26,403.00	3.78%
1/9/2012	CITYROAD	NW84THAVE	20	32,681.00	SqFt	63.19	\$65,362.00	3.00%
1/9/2012	CITYROAD	NW88THAVE	20	18,985.00	SqFt	81.6	\$28,478.00	3.00%
1/9/2012	CITYROAD	NW88THCT	30	13,466.00	SqFt	60.76	\$26,932.00	3.00%
1/9/2012	CITYROAD	NW89THCT	50	18,990.00	SqFt	88.25	\$37,979.00	3.00%
1/9/2012	CITYROAD	NW89THCT	60	15,375.00	SqFt	86.31	\$30,750.00	3.00%
1/9/2012	CITYROAD	NW89THPL	20	18,244.00	SqFt	91.21	\$36,489.00	3.00%
1/9/2012	CITYROAD	NW89THPL	40	2,043.00	SqFt	71.89	\$4,086.00	8.11%
1/9/2013	CITYROAD	NW100THAVE	30	4,729.00	SqFt	63.33	\$9,743.00	3.00%
1/9/2013	CITYROAD	NW102NDAVE	70	12,161.00	SqFt	64.59	\$25,051.00	3.00%
1/9/2013	CITYROAD	NW102NDAVE	110	6,111.00	SqFt	60.09	\$12,588.00	3.00%

RECOMMENDED PROJECTS FOR CRITICAL PCI METHOD

1/9/2013	CITYROAD	NW102NDAVE	140	2,345.00	SqFt	68.44	\$4,831.00	3.00%
1/9/2013	CITYROAD	NW102NDAVE	150	5,159.00	SqFt	68.44	\$10,627.00	3.00%
1/9/2013	CITYROAD	NW102NDAVE	210	22,270.00	SqFt	86.33	\$34,407.00	3.00%
1/9/2013	CITYROAD	NW104THAVE	30	6,721.00	SqFt	65.02	\$13,845.00	3.00%
1/9/2013	CITYROAD	NW108THAVE	20	19,232.00	SqFt	85.02	\$29,713.00	3.00%
1/9/2013	CITYROAD	NW112THAVE	210	4,807.00	SqFt	66.7	\$9,902.00	3.00%
1/9/2013	CITYROAD	NW114THAVE	120	15,358.00	SqFt	63.36	\$31,638.00	3.00%
1/9/2013	CITYROAD	NW114THAVE	150	8,365.00	SqFt	63.36	\$17,231.00	3.00%
1/9/2013	CITYROAD	NW114THAVE	310	11,948.00	SqFt	61.74	\$24,614.00	3.00%
1/9/2013	CITYROAD	NW177HST	20	19,727.00	SqFt	51.93	\$40,637.00	3.00%
1/9/2013	CITYROAD	NW20THST	10	28,721.00	SqFt	62.22	\$59,165.00	3.00%
1/9/2013	CITYROAD	NW30THTER	10	40,305.00	SqFt	65.02	\$83,028.00	3.00%
1/9/2013	CITYROAD	NW315TTER	20	4,345.00	SqFt	57.93	\$8,951.00	3.00%
1/9/2013	CITYROAD	NW33RDST	110	40,026.00	SqFt	68.46	\$82,453.00	3.00%
1/9/2013	CITYROAD	NW52NDST	20	21,745.00	SqFt	88.76	\$33,596.00	3.00%
1/9/2013	CITYROAD	NW52NDST	130	2,335.00	SqFt	57.93	\$4,811.00	3.00%
1/9/2013	CITYROAD	NW52NDST	170	6,944.00	SqFt	68.61	\$14,305.00	3.00%
1/9/2013	CITYROAD	NW52NDTER	10	3,248.00	SqFt	57.93	\$6,690.00	3.00%
1/9/2013	CITYROAD	NW54THST	20	13,355.00	SqFt	86.27	\$20,634.00	3.00%
1/9/2013	CITYROAD	NW55THST	20	13,244.00	SqFt	85.35	\$20,462.00	3.00%
1/9/2013	CITYROAD	NW79THAVE	340	22,395.00	SqFt	65.02	\$46,133.00	3.00%
1/9/2013	CITYROAD	NW82NDAVE	120	21,628.00	SqFt	62.26	\$44,553.00	3.00%
1/9/2013	CITYROAD	NW82NDAVE	150	10,160.00	SqFt	69.95	\$20,929.00	3.00%
1/9/2013	CITYROAD	NW84THAVE	80	8,045.00	SqFt	84.45	\$12,430.00	3.00%
1/9/2013	CITYROAD	NW84THAVE	100	2,879.00	SqFt	57.93	\$5,931.00	3.00%
1/9/2013	CITYROAD	NW84THAVE	150	14,818.00	SqFt	66.72	\$30,525.00	3.00%
1/9/2013	CITYROAD	NW88THCT	10	20,285.00	SqFt	63.8	\$41,787.00	3.00%
1/9/2013	CITYROAD	NW92NDAVE	10	18,868.00	SqFt	63.34	\$38,868.00	3.00%
1/9/2014	CITYROAD	NW100THAVE	20	4,741.00	SqFt	51.24	\$10,059.00	16.91%
1/9/2014	CITYROAD	NW102NDAVE	160	5,822.00	SqFt	65.57	\$12,353.00	3.00%
1/9/2014	CITYROAD	NW114THAVE	20	8,926.00	SqFt	45	\$18,939.00	205.78%
1/9/2014	CITYROAD	NW114THAVE	30	12,018.00	SqFt	45	\$25,501.00	205.78%
1/9/2014	CITYROAD	NW114THAVE	40	11,430.00	SqFt	45	\$24,253.00	205.78%
1/9/2014	CITYROAD	NW114THAVE	140	14,879.00	SqFt	45	\$31,569.00	205.78%
1/9/2014	CITYROAD	NW114THAVE	240	8,832.00	SqFt	45	\$18,739.00	205.78%
1/9/2014	CITYROAD	NW114THAVE	300	18,548.00	SqFt	51.3	\$39,355.00	15.05%
1/9/2014	CITYROAD	NW15THST	30	16,374.00	SqFt	67.54	\$34,741.00	3.00%
1/9/2014	CITYROAD	NW23RDST	10	28,622.00	SqFt	67.54	\$60,730.00	3.00%
1/9/2014	CITYROAD	NW25THTER	10	6,612.00	SqFt	51.24	\$14,030.00	16.91%
1/9/2014	CITYROAD	NW33RDST	20	18,676.00	SqFt	68.54	\$39,626.00	3.00%
1/9/2014	CITYROAD	NW33RDST	70	14,418.00	SqFt	47.06	\$30,593.00	143.67%
1/9/2014	CITYROAD	NW33RDST	90	12,641.00	SqFt	49.15	\$26,822.00	80.17%
1/9/2014	CITYROAD	NW38THST	20	20,628.00	SqFt	47.06	\$43,769.00	143.67%
1/9/2014	CITYROAD	NW53RDST	140	8,355.00	SqFt	47.02	\$17,728.00	144.60%
1/9/2014	CITYROAD	NW54THST	40	26,373.00	SqFt	49.13	\$55,957.00	81.10%
1/9/2014	CITYROAD	NW77THCT	20	6,624.00	SqFt	42.92	\$14,054.00	234.75%
1/9/2014	CITYROAD	NW77THCT	30	6,911.00	SqFt	47.02	\$14,665.00	144.60%
1/9/2014	CITYROAD	NW79THAVE	170	19,520.00	SqFt	45	\$41,417.00	205.78%
1/9/2014	CITYROAD	NW82NDAVE	50	24,586.00	SqFt	66.64	\$52,166.00	3.00%
1/9/2014	CITYROAD	NW82NDAVE	140	16,424.00	SqFt	46.39	\$34,848.00	65.34%
1/9/2014	CITYROAD	NW84THAVE	10	18,117.00	SqFt	51.3	\$38,441.00	15.05%
1/9/2014	CITYROAD	NW84THAVE	140	10,472.00	SqFt	51.3	\$22,219.00	15.05%
1/9/2014	CITYROAD	NW84THAVE	170	13,371.00	SqFt	44.96	\$28,371.00	206.94%
1/9/2014	CITYROAD	NW99THAVE	50	37,184.00	SqFt	49.37	\$78,896.00	83.42%
1/9/2015	CITYROAD	NW102NDAVE	190	14,723.00	SqFt	65.15	\$32,176.00	3.00%
1/9/2015	CITYROAD	NW109THAVE	70	31,548.00	SqFt	68.3	\$68,946.00	3.00%
1/9/2015	CITYROAD	NW114THAVE	10	4,168.00	SqFt	16.04	\$29,607.00	3.00%
1/9/2015	CITYROAD	NW114THAVE	160	12,432.00	SqFt	68.31	\$27,169.00	3.00%
1/9/2015	CITYROAD	NW114THAVE	230	11,702.00	SqFt	28.62	\$83,114.00	3.00%
1/9/2015	CITYROAD	NW215TTER	20	20,026.00	SqFt	69.37	\$43,766.00	3.00%
1/9/2015	CITYROAD	NW40THST	10	15,327.00	SqFt	28.62	\$108,865.00	3.00%
1/9/2015	CITYROAD	NW50THST	60	21,930.00	SqFt	28.57	\$155,760.00	3.00%
1/9/2015	CITYROAD	NW56THST	30	29,143.00	SqFt	28.57	\$206,996.00	3.00%
1/9/2015	CITYROAD	NW82NDAVE	90	34,473.00	SqFt	69.73	\$75,338.00	3.00%
1/9/2016	CITYROAD	NW102NDAVE	240	24,098.00	SqFt	57.94	\$54,244.00	3.00%
1/9/2016	CITYROAD	NW114THAVE	220	2,851.00	SqFt	57.94	\$6,419.00	3.00%
1/9/2016	CITYROAD	NW117THAVE	40	54,821.00	SqFt	6.93	\$401,058.00	3.00%
1/9/2016	CITYROAD	NW66THST	10	31,157.00	SqFt	57.94	\$70,135.00	3.00%
1/9/2016	CITYROAD	NW82NDAVE	40	23,366.00	SqFt	13.86	\$170,943.00	3.00%
1/9/2016	CITYROAD	NW82NDAVE	180	22,529.00	SqFt	67.62	\$50,713.00	3.00%
1/9/2016	CITYROAD	NW89THCT	10	6,549.00	SqFt	10.17	\$47,908.00	3.00%

RECOMMENDED PROJECTS FOR MINIMUM PCI METHOD

Year	NetworkID	BranchID	SectionID	True Area	Area Units	PCI Before	Cost	Delay Penalty
1/9/2012	CITYROAD	NW100THAVE	20	4,741.00	SqFt	66.5	\$9,481.00	3.00%
1/9/2012	CITYROAD	NW100THAVE	30	4,729.00	SqFt	68.75	\$9,459.00	3.00%
1/9/2012	CITYROAD	NW100THAVE	60	4,571.00	SqFt	78.6	\$7,176.00	6.44%
1/9/2012	CITYROAD	NW102NDAVE	70	12,161.00	SqFt	69.69	\$24,322.00	3.00%
1/9/2012	CITYROAD	NW102NDAVE	110	6,111.00	SqFt	66.5	\$12,222.00	3.00%
1/9/2012	CITYROAD	NW102NDAVE	130	5,818.00	SqFt	54.33	\$11,636.00	3.00%
1/9/2012	CITYROAD	NW102NDAVE	140	2,345.00	SqFt	72.18	\$4,434.00	8.94%
1/9/2012	CITYROAD	NW102NDAVE	150	5,159.00	SqFt	72.18	\$9,755.00	8.94%
1/9/2012	CITYROAD	NW102NDAVE	160	5,822.00	SqFt	73.34	\$10,671.00	11.63%
1/9/2012	CITYROAD	NW102NDAVE	190	14,723.00	SqFt	77.23	\$24,123.00	12.77%
1/9/2012	CITYROAD	NW102NDAVE	240	24,098.00	SqFt	69.94	\$48,195.00	3.00%
1/9/2012	CITYROAD	NW104THAVE	10	11,007.00	SqFt	75.76	\$18,843.00	6.97%
1/9/2012	CITYROAD	NW104THAVE	30	6,721.00	SqFt	69.91	\$13,441.00	3.00%
1/9/2012	CITYROAD	NW109THAVE	70	31,548.00	SqFt	75.76	\$54,010.00	6.97%
1/9/2012	CITYROAD	NW112THAVE	70	5,053.00	SqFt	77.71	\$8,158.00	5.23%
1/9/2012	CITYROAD	NW112THAVE	210	4,807.00	SqFt	71.04	\$9,364.00	5.75%
1/9/2012	CITYROAD	NW112THAVE	270	9,151.00	SqFt	76.87	\$15,159.00	3.78%
1/9/2012	CITYROAD	NW114THAVE	10	4,168.00	SqFt	56.57	\$8,337.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	20	8,926.00	SqFt	63.19	\$17,852.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	30	12,018.00	SqFt	63.19	\$24,037.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	40	11,430.00	SqFt	63.19	\$22,861.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	120	15,358.00	SqFt	68.78	\$30,716.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	140	14,879.00	SqFt	63.19	\$29,757.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	150	8,365.00	SqFt	68.78	\$16,729.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	160	12,432.00	SqFt	75.76	\$21,284.00	6.97%
1/9/2012	CITYROAD	NW114THAVE	210	13,833.00	SqFt	60.98	\$27,666.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	220	2,851.00	SqFt	69.94	\$5,703.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	230	11,702.00	SqFt	62.08	\$23,403.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	240	8,832.00	SqFt	63.19	\$17,663.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	280	15,298.00	SqFt	78.61	\$24,011.00	6.48%
1/9/2012	CITYROAD	NW114THAVE	300	18,548.00	SqFt	66.53	\$37,096.00	3.00%
1/9/2012	CITYROAD	NW114THAVE	310	11,948.00	SqFt	67.65	\$23,897.00	3.00%
1/9/2012	CITYROAD	NW117THAVE	20	38,184.00	SqFt	60.96	\$76,368.00	3.00%
1/9/2012	CITYROAD	NW117THAVE	40	54,821.00	SqFt	60.94	\$109,641.00	3.00%
1/9/2012	CITYROAD	NW13THTER	20	22,573.00	SqFt	75.32	\$39,142.00	12.24%
1/9/2012	CITYROAD	NW13THTER	30	15,911.00	SqFt	75.32	\$27,591.00	12.24%
1/9/2012	CITYROAD	NW15THST	10	12,619.00	SqFt	60.98	\$25,237.00	3.00%
1/9/2012	CITYROAD	NW15THST	30	16,374.00	SqFt	75.32	\$28,392.00	12.24%
1/9/2012	CITYROAD	NW17THST	20	19,727.00	SqFt	54.94	\$39,453.00	3.00%
1/9/2012	CITYROAD	NW20THST	10	28,721.00	SqFt	68.52	\$57,442.00	3.00%
1/9/2012	CITYROAD	NW21STST	30	20,351.00	SqFt	57.67	\$40,702.00	3.00%
1/9/2012	CITYROAD	NW21STST	50	26,289.00	SqFt	47.84	\$52,578.00	120.96%
1/9/2012	CITYROAD	NW21STTER	10	9,381.00	SqFt	79.51	\$14,301.00	7.53%
1/9/2012	CITYROAD	NW21STTER	20	20,026.00	SqFt	77.68	\$32,362.00	8.04%
1/9/2012	CITYROAD	NW23RDST	10	28,622.00	SqFt	75.32	\$49,630.00	12.24%
1/9/2012	CITYROAD	NW25THTER	10	6,612.00	SqFt	66.5	\$13,225.00	3.00%
1/9/2012	CITYROAD	NW27THST	30	50,440.00	SqFt	77.71	\$81,436.00	5.23%
1/9/2012	CITYROAD	NW27THST	60	33,710.00	SqFt	77.71	\$54,426.00	5.23%
1/9/2012	CITYROAD	NW28THTER	30	13,132.00	SqFt	79.51	\$20,019.00	7.53%
1/9/2012	CITYROAD	NW29THST	20	39,410.00	SqFt	76.87	\$65,283.00	3.78%
1/9/2012	CITYROAD	NW29THST	90	18,984.00	SqFt	79.51	\$28,942.00	7.53%
1/9/2012	CITYROAD	NW30THTER	10	40,305.00	SqFt	69.91	\$80,610.00	3.00%
1/9/2012	CITYROAD	NW30THTER	30	17,664.00	SqFt	77.71	\$28,519.00	5.23%
1/9/2012	CITYROAD	NW31STTER	20	4,345.00	SqFt	60.94	\$8,691.00	3.00%
1/9/2012	CITYROAD	NW33RDST	20	18,676.00	SqFt	74.54	\$33,112.00	9.71%
1/9/2012	CITYROAD	NW33RDST	70	14,418.00	SqFt	64.3	\$28,837.00	3.00%
1/9/2012	CITYROAD	NW33RDST	90	12,641.00	SqFt	65.41	\$25,282.00	3.00%
1/9/2012	CITYROAD	NW33RDST	110	40,026.00	SqFt	72.19	\$75,668.00	8.97%

RECOMMENDED PROJECTS FOR MINIMUM PCI METHOD

1/9/2012	CITYROAD	NW35THLN	10	41,860.00	SqFt	78.61	\$65,700.00	6.48%
1/9/2012	CITYROAD	NW37THST	10	13,206.00	SqFt	48.93	\$26,413.00	87.82%
1/9/2012	CITYROAD	NW37THST	20	5,597.00	SqFt	46.76	\$11,193.00	153.64%
1/9/2012	CITYROAD	NW38THST	10	13,274.00	SqFt	31.62	\$76,603.00	16.01%
1/9/2012	CITYROAD	NW38THST	20	20,628.00	SqFt	64.3	\$41,257.00	3.00%
1/9/2012	CITYROAD	NW40THST	10	15,327.00	SqFt	62.08	\$30,654.00	3.00%
1/9/2012	CITYROAD	NW40THST	20	10,592.00	SqFt	72.19	\$20,024.00	8.97%
1/9/2012	CITYROAD	NW41STST	10	7,748.00	SqFt	7.98	\$50,363.00	3.00%
1/9/2012	CITYROAD	NW41STST	20	21,852.00	SqFt	52.17	\$43,704.00	3.00%
1/9/2012	CITYROAD	NW41STST	40	23,840.00	SqFt	60.96	\$47,679.00	3.00%
1/9/2012	CITYROAD	NW41STST	80	33,950.00	SqFt	41.3	\$67,901.00	234.75%
1/9/2012	CITYROAD	NW50THST	60	21,930.00	SqFt	62.06	\$43,859.00	3.00%
1/9/2012	CITYROAD	NW52NDST	70	10,753.00	SqFt	72.19	\$20,328.00	8.97%
1/9/2012	CITYROAD	NW52NDST	120	6,444.00	SqFt	79.51	\$9,825.00	7.53%
1/9/2012	CITYROAD	NW52NDST	130	2,335.00	SqFt	60.94	\$4,671.00	3.00%
1/9/2012	CITYROAD	NW52NDST	150	13,431.00	SqFt	72.19	\$25,390.00	8.97%
1/9/2012	CITYROAD	NW52NDST	170	6,944.00	SqFt	72.92	\$12,874.00	11.11%
1/9/2012	CITYROAD	NW52NDST	230	6,039.00	SqFt	73.35	\$11,066.00	11.63%
1/9/2012	CITYROAD	NW52NDTER	10	3,248.00	SqFt	60.94	\$6,495.00	3.00%
1/9/2012	CITYROAD	NW53RDST	20	13,540.00	SqFt	72.19	\$25,598.00	8.97%
1/9/2012	CITYROAD	NW53RDST	100	11,401.00	SqFt	76.87	\$18,885.00	3.78%
1/9/2012	CITYROAD	NW53RDST	140	8,355.00	SqFt	64.28	\$16,710.00	3.00%
1/9/2012	CITYROAD	NW54THST	10	26,495.00	SqFt	55.45	\$52,990.00	3.00%
1/9/2012	CITYROAD	NW54THST	40	26,373.00	SqFt	65.4	\$52,745.00	3.00%
1/9/2012	CITYROAD	NW54THST	50	26,276.00	SqFt	71.04	\$51,186.00	5.75%
1/9/2012	CITYROAD	NW56THST	20	28,912.00	SqFt	73.35	\$52,981.00	11.63%
1/9/2012	CITYROAD	NW56THST	30	29,143.00	SqFt	62.06	\$58,286.00	3.00%
1/9/2012	CITYROAD	NW66THST	10	31,157.00	SqFt	69.94	\$62,314.00	3.00%
1/9/2012	CITYROAD	NW77THCT	10	6,416.00	SqFt	59.85	\$12,833.00	3.00%
1/9/2012	CITYROAD	NW77THCT	20	6,624.00	SqFt	62.06	\$13,247.00	3.00%
1/9/2012	CITYROAD	NW77THCT	30	6,911.00	SqFt	64.28	\$13,823.00	3.00%
1/9/2012	CITYROAD	NW77THCT	40	6,953.00	SqFt	74.53	\$12,331.00	9.68%
1/9/2012	CITYROAD	NW77THCT	50	19,197.00	SqFt	69.89	\$38,395.00	3.00%
1/9/2012	CITYROAD	NW78THAVE	30	59,595.00	SqFt	66.32	\$119,189.00	3.00%
1/9/2012	CITYROAD	NW78THAVE	80	5,842.00	SqFt	47.84	\$11,684.00	120.96%
1/9/2012	CITYROAD	NW79THAVE	160	19,833.00	SqFt	59.87	\$39,665.00	3.00%
1/9/2012	CITYROAD	NW79THAVE	170	19,520.00	SqFt	63.19	\$39,039.00	3.00%
1/9/2012	CITYROAD	NW79THAVE	340	22,395.00	SqFt	69.91	\$44,789.00	3.00%
1/9/2012	CITYROAD	NW79THAVE	350	16,329.00	SqFt	78.61	\$25,628.00	6.48%
1/9/2012	CITYROAD	NW82NDAVE	10	15,939.00	SqFt	76.87	\$26,403.00	3.78%
1/9/2012	CITYROAD	NW82NDAVE	40	23,366.00	SqFt	52.81	\$46,733.00	3.00%
1/9/2012	CITYROAD	NW82NDAVE	50	24,586.00	SqFt	75.07	\$42,939.00	14.00%
1/9/2012	CITYROAD	NW82NDAVE	90	34,473.00	SqFt	79.41	\$52,726.00	11.18%
1/9/2012	CITYROAD	NW82NDAVE	120	21,628.00	SqFt	68.55	\$43,256.00	3.00%
1/9/2012	CITYROAD	NW82NDAVE	140	16,424.00	SqFt	61.21	\$32,847.00	3.00%
1/9/2012	CITYROAD	NW82NDAVE	150	10,160.00	SqFt	74	\$18,287.00	14.44%
1/9/2012	CITYROAD	NW84THAVE	10	18,117.00	SqFt	66.53	\$36,235.00	3.00%
1/9/2012	CITYROAD	NW84THAVE	20	32,681.00	SqFt	63.19	\$65,362.00	3.00%
1/9/2012	CITYROAD	NW84THAVE	100	2,879.00	SqFt	60.94	\$5,758.00	3.00%
1/9/2012	CITYROAD	NW84THAVE	140	10,472.00	SqFt	66.53	\$20,944.00	3.00%
1/9/2012	CITYROAD	NW84THAVE	150	14,818.00	SqFt	71.05	\$28,858.00	5.78%
1/9/2012	CITYROAD	NW84THAVE	170	13,371.00	SqFt	63.17	\$26,742.00	3.00%
1/9/2012	CITYROAD	NW86THST	10	8,102.00	SqFt	79.51	\$12,352.00	7.53%
1/9/2012	CITYROAD	NW88THCT	10	20,285.00	SqFt	69.64	\$40,570.00	3.00%
1/9/2012	CITYROAD	NW88THCT	30	13,466.00	SqFt	60.76	\$26,932.00	3.00%
1/9/2012	CITYROAD	NW89THCT	10	6,549.00	SqFt	62.97	\$13,097.00	3.00%
1/9/2012	CITYROAD	NW89THPL	40	2,043.00	SqFt	71.89	\$3,893.00	8.11%
1/9/2012	CITYROAD	NW92NDAVE	10	18,868.00	SqFt	68.76	\$37,736.00	3.00%
1/9/2012	CITYROAD	NW99THAVE	20	19,968.00	SqFt	59.87	\$39,935.00	3.00%

RECOMMENDED PROJECTS FOR MINIMUM PCI METHOD

1/9/2012	CITYROAD	NW99THAVE	50	37,184.00	SqFt	66.29	\$74,367.00	3.00%
1/9/2013	CITYROAD	NW102NDPL	10	36,692.00	SqFt	79.55	\$57,540.00	7.57%
1/9/2013	CITYROAD	NW112THAVE	60	7,981.00	SqFt	79.56	\$12,512.00	7.60%
1/9/2013	CITYROAD	NW112THAVE	100	5,500.00	SqFt	78.84	\$8,826.00	6.74%
1/9/2013	CITYROAD	NW114THAVE	180	11,210.00	SqFt	79.56	\$17,573.00	7.57%
1/9/2013	CITYROAD	NW21STST	80	13,694.00	SqFt	79.55	\$21,474.00	7.57%
1/9/2013	CITYROAD	NW21STST	90	25,148.00	SqFt	79.55	\$39,436.00	7.57%
1/9/2013	CITYROAD	NW27THST	40	31,924.00	SqFt	78.84	\$51,230.00	6.74%
1/9/2013	CITYROAD	NW29THTER	10	36,036.00	SqFt	79.56	\$56,492.00	7.57%
1/9/2013	CITYROAD	NW33RDST	10	21,187.00	SqFt	78.4	\$34,480.00	11.93%
1/9/2013	CITYROAD	NW50THST	10	26,440.00	SqFt	78.84	\$42,429.00	6.74%
1/9/2013	CITYROAD	NW52NDTER	20	26,968.00	SqFt	78.84	\$43,276.00	6.74%
1/9/2013	CITYROAD	NW53RDST	120	4,295.00	SqFt	79.56	\$6,733.00	7.60%
1/9/2013	CITYROAD	NW82NDAVE	80	16,777.00	SqFt	79.86	\$26,042.00	10.83%
1/9/2013	CITYROAD	NW82NDAVE	180	22,529.00	SqFt	78.42	\$36,641.00	11.94%
1/9/2013	CITYROAD	NW89THCT	30	16,602.00	SqFt	79.55	\$26,035.00	7.57%
1/9/2014	CITYROAD	NW100THAVE	50	19,681.00	SqFt	84.5	\$31,319.00	3.00%
1/9/2014	CITYROAD	NW102NDAVE	200	3,656.00	SqFt	82.57	\$5,818.00	3.00%
1/9/2014	CITYROAD	NW102NDAVE	210	22,270.00	SqFt	85.26	\$35,439.00	3.00%
1/9/2014	CITYROAD	NW104THAVE	20	25,518.00	SqFt	78.75	\$42,300.00	6.63%
1/9/2014	CITYROAD	NW105THAVE	10	12,582.00	SqFt	85.4	\$20,022.00	3.00%
1/9/2014	CITYROAD	NW108THAVE	20	19,232.00	SqFt	83.12	\$30,605.00	3.00%
1/9/2014	CITYROAD	NW109THAVE	10	27,237.00	SqFt	78.75	\$45,150.00	6.63%
1/9/2014	CITYROAD	NW110THAVE	10	4,539.00	SqFt	79.96	\$7,232.00	8.04%
1/9/2014	CITYROAD	NW112THAVE	10	9,072.00	SqFt	84.51	\$14,437.00	3.00%
1/9/2014	CITYROAD	NW112THAVE	40	5,747.00	SqFt	84.51	\$9,145.00	3.00%
1/9/2014	CITYROAD	NW112THAVE	80	13,405.00	SqFt	78.75	\$22,221.00	6.63%
1/9/2014	CITYROAD	NW112THAVE	140	4,737.00	SqFt	78.75	\$7,853.00	6.63%
1/9/2014	CITYROAD	NW112THAVE	240	13,439.00	SqFt	84.51	\$21,387.00	3.00%
1/9/2014	CITYROAD	NW114THAVE	90	18,876.00	SqFt	85.4	\$30,039.00	3.00%
1/9/2014	CITYROAD	NW114THAVE	100	8,903.00	SqFt	82.07	\$14,168.00	3.00%
1/9/2014	CITYROAD	NW114THAVE	170	6,659.00	SqFt	78.75	\$11,038.00	6.63%
1/9/2014	CITYROAD	NW117THAVE	30	15,095.00	SqFt	79.33	\$24,557.00	7.30%
1/9/2014	CITYROAD	NW13THTER	10	6,157.00	SqFt	83.83	\$9,798.00	3.00%
1/9/2014	CITYROAD	NW14THST	20	22,757.00	SqFt	79.97	\$36,251.00	8.04%
1/9/2014	CITYROAD	NW14THST	30	46,443.00	SqFt	79.34	\$75,533.00	7.33%
1/9/2014	CITYROAD	NW15THST	20	5,730.00	SqFt	79.79	\$9,182.00	4.40%
1/9/2014	CITYROAD	NW17THST	10	28,781.00	SqFt	85.39	\$45,801.00	3.00%
1/9/2014	CITYROAD	NW17THST	30	44,154.00	SqFt	78.74	\$73,216.00	6.59%
1/9/2014	CITYROAD	NW17THST	40	23,476.00	SqFt	83.66	\$37,359.00	3.00%
1/9/2014	CITYROAD	NW18THTER	10	41,619.00	SqFt	85.38	\$66,231.00	3.00%
1/9/2014	CITYROAD	NW18THTER	20	14,976.00	SqFt	84.58	\$23,832.00	3.00%
1/9/2014	CITYROAD	NW21STST	10	16,212.00	SqFt	81.33	\$25,799.00	4.65%
1/9/2014	CITYROAD	NW21STST	20	11,758.00	SqFt	79.33	\$19,129.00	7.33%
1/9/2014	CITYROAD	NW21STST	70	5,772.00	SqFt	80.62	\$9,185.00	6.50%
1/9/2014	CITYROAD	NW21STST	100	5,222.00	SqFt	81.81	\$8,310.00	3.00%
1/9/2014	CITYROAD	NW21STST	120	9,058.00	SqFt	81.81	\$14,414.00	3.00%
1/9/2014	CITYROAD	NW23RDST	20	22,709.00	SqFt	80.62	\$36,138.00	6.50%
1/9/2014	CITYROAD	NW24THTER	10	21,529.00	SqFt	80.21	\$34,261.00	4.30%
1/9/2014	CITYROAD	NW26THST	10	13,716.00	SqFt	85.38	\$21,828.00	3.00%
1/9/2014	CITYROAD	NW26THST	20	30,935.00	SqFt	80.62	\$49,229.00	6.50%
1/9/2014	CITYROAD	NW26THST	30	12,944.00	SqFt	79.33	\$21,059.00	7.33%
1/9/2014	CITYROAD	NW26THST	40	13,036.00	SqFt	79.96	\$20,773.00	8.04%
1/9/2014	CITYROAD	NW26THST	50	13,311.00	SqFt	83.65	\$21,183.00	3.00%
1/9/2014	CITYROAD	NW26THST	60	12,994.00	SqFt	81.33	\$20,679.00	4.65%
1/9/2014	CITYROAD	NW27THST	50	13,027.00	SqFt	84.5	\$20,731.00	3.00%
1/9/2014	CITYROAD	NW27THST	70	11,553.00	SqFt	79.33	\$18,795.00	7.33%
1/9/2014	CITYROAD	NW27THST	80	13,474.00	SqFt	80.62	\$21,442.00	6.50%
1/9/2014	CITYROAD	NW27THST	120	12,988.00	SqFt	78.74	\$21,536.00	6.59%

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1/9/2014	CITYROAD	NW29THST	30	17,267.00	SqFt	80.62	\$27,478.00	6.50%
1/9/2014	CITYROAD	NW29THST	50	28,032.00	SqFt	85.38	\$44,609.00	3.00%
1/9/2014	CITYROAD	NW29THST	60	21,023.00	SqFt	82.44	\$33,456.00	3.00%
1/9/2014	CITYROAD	NW29THST	80	39,117.00	SqFt	85.4	\$62,248.00	3.00%
1/9/2014	CITYROAD	NW30THTER	20	12,088.00	SqFt	79.34	\$19,660.00	7.33%
1/9/2014	CITYROAD	NW31STST	10	29,029.00	SqFt	84.5	\$46,196.00	3.00%
1/9/2014	CITYROAD	NW33RDST	30	19,184.00	SqFt	84.51	\$30,528.00	3.00%
1/9/2014	CITYROAD	NW33RDST	60	13,917.00	SqFt	78.75	\$23,070.00	6.63%
1/9/2014	CITYROAD	NW33RDST	120	18,059.00	SqFt	85.39	\$28,738.00	3.00%
1/9/2014	CITYROAD	NW33RDST	150	10,119.00	SqFt	84.52	\$16,104.00	3.00%
1/9/2014	CITYROAD	NW33RDST	170	18,218.00	SqFt	83.66	\$28,991.00	3.00%
1/9/2014	CITYROAD	NW33RDST	180	39,797.00	SqFt	81.14	\$63,331.00	5.47%
1/9/2014	CITYROAD	NW36THTER	10	14,919.00	SqFt	83.65	\$23,742.00	3.00%
1/9/2014	CITYROAD	NW50THST	30	26,988.00	SqFt	83.66	\$42,947.00	3.00%
1/9/2014	CITYROAD	NW52NDST	160	6,883.00	SqFt	85.95	\$10,953.00	3.00%
1/9/2014	CITYROAD	NW52NDST	210	24,122.00	SqFt	79.43	\$39,116.00	11.15%
1/9/2014	CITYROAD	NW53RDST	90	19,322.00	SqFt	82.07	\$30,749.00	3.00%
1/9/2014	CITYROAD	NW53RDST	110	32,164.00	SqFt	85.4	\$51,184.00	3.00%
1/9/2014	CITYROAD	NW53RDST	130	4,967.00	SqFt	80.63	\$7,904.00	6.47%
1/9/2014	CITYROAD	NW53RDTER	10	24,809.00	SqFt	78.75	\$41,124.00	6.63%
1/9/2014	CITYROAD	NW54THST	20	13,355.00	SqFt	83.66	\$21,253.00	3.00%
1/9/2014	CITYROAD	NW54THST	30	10,884.00	SqFt	82.07	\$17,320.00	3.00%
1/9/2014	CITYROAD	NW55THST	10	10,854.00	SqFt	83.66	\$17,272.00	3.00%
1/9/2014	CITYROAD	NW55THST	20	13,244.00	SqFt	82.85	\$21,076.00	3.00%
1/9/2014	CITYROAD	NW56THST	10	29,212.00	SqFt	79.33	\$47,524.00	7.30%
1/9/2014	CITYROAD	NW56THST	40	14,634.00	SqFt	79.96	\$23,319.00	8.04%
1/9/2014	CITYROAD	NW78THAVE	10	7,199.00	SqFt	79.33	\$11,712.00	7.30%
1/9/2014	CITYROAD	NW78THAVE	70	7,277.00	SqFt	82.85	\$11,579.00	3.00%
1/9/2014	CITYROAD	NW82NDAVE	20	16,236.00	SqFt	82.57	\$25,837.00	3.00%
1/9/2014	CITYROAD	NW82NDAVE	30	15,832.00	SqFt	83.66	\$25,195.00	3.00%
1/9/2014	CITYROAD	NW82NDAVE	100	20,885.00	SqFt	85.26	\$33,236.00	3.00%
1/9/2014	CITYROAD	NW82NDAVE	160	5,806.00	SqFt	85.96	\$9,239.00	3.00%
1/9/2014	CITYROAD	NW84THAVE	40	39,571.00	SqFt	82.07	\$62,972.00	3.00%
1/9/2014	CITYROAD	NW84THAVE	50	4,449.00	SqFt	81.33	\$7,080.00	4.61%
1/9/2014	CITYROAD	NW84THAVE	70	32,772.00	SqFt	80.62	\$52,151.00	6.50%
1/9/2014	CITYROAD	NW84THAVE	80	8,045.00	SqFt	82.07	\$12,803.00	3.00%
1/9/2014	CITYROAD	NW84THAVE	90	22,450.00	SqFt	81.33	\$35,726.00	4.65%
1/9/2014	CITYROAD	NW84THAVE	130	12,726.00	SqFt	85.39	\$20,251.00	3.00%
1/9/2014	CITYROAD	NW84THAVE	160	24,225.00	SqFt	84.5	\$38,550.00	3.00%
1/9/2014	CITYROAD	NW86THAVE	10	12,162.00	SqFt	80.62	\$19,354.00	6.50%
1/9/2014	CITYROAD	NW86THST	30	17,895.00	SqFt	85.4	\$28,477.00	3.00%
1/9/2014	CITYROAD	NW86THST	50	8,196.00	SqFt	78.75	\$13,586.00	6.63%
1/9/2014	CITYROAD	NW87THCT	10	17,076.00	SqFt	80.62	\$27,174.00	6.50%
1/9/2014	CITYROAD	NW88THAVE	20	18,985.00	SqFt	79.79	\$30,424.00	4.40%
1/9/2014	CITYROAD	NW88THCT	20	14,460.00	SqFt	81.23	\$23,011.00	3.00%
1/9/2014	CITYROAD	NW89THCT	20	20,993.00	SqFt	79.34	\$34,142.00	7.33%
1/9/2014	CITYROAD	NW89THCT	50	18,990.00	SqFt	83.83	\$30,219.00	3.00%
1/9/2014	CITYROAD	NW89THCT	60	15,375.00	SqFt	82.44	\$24,467.00	3.00%
1/9/2014	CITYROAD	NW91STAVE	10	17,737.00	SqFt	81.34	\$28,225.00	4.61%

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1/9/2014	CITYROAD	NW92NDAVE	30	12,764.00	SqFt	81.33	\$20,313.00	4.65%
1/9/2014	CITYROAD	NW92NDAVE	40	16,652.00	SqFt	83.65	\$26,500.00	3.00%
1/9/2014	CITYROAD	NW92NDAVE	50	12,709.00	SqFt	80.62	\$20,225.00	6.50%
1/9/2014	CITYROAD	NW99THAVE	90	8,935.00	SqFt	82.08	\$14,218.00	3.00%
1/9/2015	CITYROAD	NW100THAVE	10	5,347.00	SqFt	83.71	\$8,764.00	3.00%
1/9/2015	CITYROAD	NW102NDAVE	90	26,224.00	SqFt	85.55	\$42,984.00	3.00%
1/9/2015	CITYROAD	NW104THCT	10	19,376.00	SqFt	84.11	\$31,759.00	3.00%
1/9/2015	CITYROAD	NW109THAVE	20	28,153.00	SqFt	83.71	\$46,146.00	3.00%
1/9/2015	CITYROAD	NW112THAVE	120	17,936.00	SqFt	83.71	\$29,399.00	3.00%
1/9/2015	CITYROAD	NW112THAVE	130	8,219.00	SqFt	85.56	\$13,472.00	3.00%
1/9/2015	CITYROAD	NW112THAVE	220	5,140.00	SqFt	83.71	\$8,425.00	3.00%
1/9/2015	CITYROAD	NW112THAVE	230	19,895.00	SqFt	85.56	\$32,611.00	3.00%
1/9/2015	CITYROAD	NW114THAVE	200	15,947.00	SqFt	84.6	\$26,139.00	3.00%
1/9/2015	CITYROAD	NW114THAVE	260	6,588.00	SqFt	83.71	\$10,798.00	3.00%
1/9/2015	CITYROAD	NW15THST	40	16,093.00	SqFt	85.7	\$26,378.00	3.00%
1/9/2015	CITYROAD	NW19THST	20	59,896.00	SqFt	85.55	\$98,175.00	3.00%
1/9/2015	CITYROAD	NW21STST	40	17,926.00	SqFt	85.55	\$29,383.00	3.00%
1/9/2015	CITYROAD	NW21STST	110	23,074.00	SqFt	85.56	\$37,820.00	3.00%
1/9/2015	CITYROAD	NW31STST	20	9,797.00	SqFt	85.56	\$16,058.00	3.00%
1/9/2015	CITYROAD	NW31STTER	10	20,529.00	SqFt	83.71	\$33,649.00	3.00%
1/9/2015	CITYROAD	NW33RDST	40	8,863.00	SqFt	83.72	\$14,528.00	3.00%
1/9/2015	CITYROAD	NW33RDST	130	37,046.00	SqFt	83.71	\$60,722.00	3.00%
1/9/2015	CITYROAD	NW33RDST	140	26,265.00	SqFt	85.55	\$43,051.00	3.00%
1/9/2015	CITYROAD	NW34THST	10	15,321.00	SqFt	83.71	\$25,113.00	3.00%
1/9/2015	CITYROAD	NW52NDST	50	2,740.00	SqFt	83.71	\$4,491.00	3.00%
1/9/2015	CITYROAD	NW53RDST	80	5,539.00	SqFt	83.71	\$9,079.00	3.00%
1/9/2015	CITYROAD	NW56THST	50	11,017.00	SqFt	85.56	\$18,058.00	3.00%
1/9/2015	CITYROAD	NW66THST	20	51,836.00	SqFt	83.71	\$84,963.00	3.00%
1/9/2015	CITYROAD	NW82NDST	10	48,605.00	SqFt	84.88	\$79,668.00	3.00%
1/9/2015	CITYROAD	NW82NDST	20	42,487.00	SqFt	85.7	\$69,640.00	3.00%
1/9/2015	CITYROAD	NW84THAVE	30	16,495.00	SqFt	83.71	\$27,037.00	3.00%
1/9/2015	CITYROAD	NW88THAVE	10	5,787.00	SqFt	84.87	\$9,486.00	3.00%
1/9/2015	CITYROAD	NW89THCT	40	13,944.00	SqFt	84.59	\$22,855.00	3.00%
1/9/2015	CITYROAD	NW89THPL	10	21,483.00	SqFt	85.7	\$35,212.00	3.00%
1/9/2015	CITYROAD	NW89THPL	20	18,244.00	SqFt	84.11	\$29,904.00	3.00%
1/9/2015	CITYROAD	NW89THPL	30	27,925.00	SqFt	84.11	\$45,771.00	3.00%
1/9/2015	CITYROAD	NW98THCT	10	36,239.00	SqFt	83.71	\$59,398.00	3.00%
1/9/2016	CITYROAD	NW102NDAVE	100	26,913.00	SqFt	85	\$45,437.00	Unknown
1/9/2016	CITYROAD	NW102NDAVE	250	1,911.00	SqFt	83.95	\$3,226.00	Unknown
1/9/2016	CITYROAD	NW102NDAVE	260	38,921.00	SqFt	85.6	\$65,710.00	Unknown
1/9/2016	CITYROAD	NW104THAVE	40	21,446.00	SqFt	83.95	\$36,206.00	Unknown
1/9/2016	CITYROAD	NW105THAVE	20	8,888.00	SqFt	85.3	\$15,006.00	Unknown
1/9/2016	CITYROAD	NW109THAVE	100	8,973.00	SqFt	83.95	\$15,148.00	Unknown
1/9/2016	CITYROAD	NW109THAVE	120	18,604.00	SqFt	85	\$31,409.00	Unknown
1/9/2016	CITYROAD	NW112THAVE	20	41,389.00	SqFt	83.95	\$69,876.00	Unknown
1/9/2016	CITYROAD	NW112THAVE	160	19,395.00	SqFt	83.95	\$32,743.00	Unknown
1/9/2016	CITYROAD	NW114THAVE	110	27,464.00	SqFt	85	\$46,367.00	Unknown
1/9/2016	CITYROAD	NW114THAVE	270	27,826.00	SqFt	83.95	\$46,979.00	Unknown
1/9/2016	CITYROAD	NW115THAVE	10	52,655.00	SqFt	83.95	\$88,896.00	Unknown
1/9/2016	CITYROAD	NW115THAVE	20	19,507.00	SqFt	85	\$32,932.00	Unknown
1/9/2016	CITYROAD	NW25THTER	20	11,840.00	SqFt	83.95	\$19,989.00	Unknown
1/9/2016	CITYROAD	NW29THST	40	14,314.00	SqFt	84.44	\$24,166.00	Unknown
1/9/2016	CITYROAD	NW50THST	70	5,776.00	SqFt	85	\$9,751.00	Unknown
1/9/2016	CITYROAD	NW52NDST	20	21,745.00	SqFt	85.07	\$36,711.00	Unknown
1/9/2016	CITYROAD	NW52NDST	30	2,255.00	SqFt	85	\$3,806.00	Unknown
1/9/2016	CITYROAD	NW52NDST	40	4,189.00	SqFt	83.95	\$7,072.00	Unknown
1/9/2016	CITYROAD	NW82NDAVE	60	10,566.00	SqFt	85.6	\$17,838.00	Unknown
1/9/2016	CITYROAD	NW82NDAVE	190	19,521.00	SqFt	85.07	\$32,956.00	Unknown
1/9/2016	CITYROAD	NW84THAVE	110	15,730.00	SqFt	85	\$26,556.00	Unknown